

ASSOCIATION OF

AMERICAN RAILROADS

Operations and Maintenance Department • Transportation Division

American Railroads Building • Washington, D. C. 20001



UNIVERSAL MACHINE LANGUAGE EQUIPMENT REGISTER (UMLER) DATA SPECIFICATION MANUAL

EFFECTIVE OCTOBER 1, 1986

INTRODUCTION

UMLER FILE

The UMLER (Universal Machine Language Equipment Register) file is a computer file which contains specific details on the internal and external dimensions, carrying capacities expressed in gallons/cubic feet capacity, equipment weight, as well as special equipment on all railcars and highway trailers and containers that are used in interchange service. The number of cars and units registered in the file is approximately 2,400,000. The UMLER file is maintained by the Transportation Division of the Association of American Railroads in Washington, D.C.

Uses of the UMLER File

The UMLER file is the industry's official source from which railroad car hire rates, private car mileage rates and trailer and container indicator are derived for monthly accounting.

The UMLER file is the source of information for publishing the cars' dimensional and capacity information in The Official Railway Equipment Register.

Numerous railroad operating offices utilize the file to determine car assignments, lengths and weights to determine train makeup and line clearances. In addition, they can determine various special characteristics of cars to fulfill the shippers' car requirements.

Many railroad traffic departments utilize the cars' tare weight and capacity information in their automated billing systems.

The AAR verifies all interchange movements reported through the TRAIN system by validating the initial and number being reported. Also, the file is used to control the movement of overage equipment and cars not meeting all requirements of FRA and AAR Mechanical Interchange rules that would restrict the interchange of a car.

Purpose of the UMLER Data Specification Manual:

This manual specifies data requirements for the proper reporting of rail cars and highway trailers and containers. UMLER is the master file from which the Car Hire and Mileage Rates and the TeleRail Automated Information Network (TRAIN II) Freight Car Master files are derived and from which Car Service ownership reports are developed. Those responsible for preparation of UMLER data must be familiar with the entire contents of this manual.

REQUIREMENTS:

The UMLER Manual, divided into six sections related to type of equipment, plus exhibits, data input requirements and is the basis for the AAR computer edit programs. Exhibit Car and Locomotive Type Codes and TOFC/COFC Type Codes, indicates the particular of the Specification Manual to be used as a guide in developing UMLER data. The minimum and maximum parameters, where shown, are the allowable limits permitted by the AAR edit program.

Edits: UMLER data will be edited. Add and change records that meet the minimum edit will be processed and errors returned to the submitting party. The submitting party will have thirty (30) days to correct the record. Records that are not corrected within thirty (30) days will have zero rates and a zero (0) indicator code inserted into the records per Car Hire and Mileage Tariff PHA 6007-Series. Add and change records that do not meet the minimum edit criteria will be rejected without processing to the records or to the file. The file will cause transactions to be rejected are listed in "Exhibit L".

If owners have equipment that is so unique it cannot pass the AAR Edit Requirements, it must be presented on the prescribed UMLER document to the AAR in advance of the equipment being placed into service. This equipment will be processed by the AAR for inclusion in the Exception Control file which allows the unique information to pass the edit and the reported information is provided to the industry (See Exhibit T). The zero rates and the zero indicator have been inserted into a record, the appropriate information must be resubmitted in addition to the corrected data field.

Tabcooses, locomotives, maintenance of way equipment and locomotives will be edited albatric and numeric information for the specific fields indicated in the heading for CAR Section I, having an asterisk (*) and on Format A. The reporting of other data fields is optional and will be edited only for alpha/numeric information.

C. Notification of Errors: The notification of error records is covered in detail in Exhibit L.

D. General Rules for Alphabetic and Numeric Field Identification: When preparing submissions, the applicable format found in the front of each Section of the Manual must be coordinated with the appropriate text for accurate reporting. Alphabetic information is left justified, e.g., A B C . . . Numeric information is right justified with high order zeroes, e.g., 0 0 2 3 4 5 .

When reporting alphabetic or alphanumeric fields and the field is not applicable to the car/unit the field should be left blank. If the field is numeric but is not applicable to the car/unit the field should be zero filled.

E. Procedures for Identifying and Removing Equipment Having Cancelled Reporting Marks:

1. Upon receipt of notice from the owner, agent or the Interstate Commerce Commission that a company having equipment registered in the UMLER file has or will cease operations, the AAR will serve notice to the owner/agent that the reporting marks will be cancelled thirty (30) days after the cessation of operations and that the transportation code 'M' will be inserted into the records. The owner/agent must delete all equipment from the UMLER file within ninety (90) days after the cancellation of the reporting mark. When, after the ninety (90) days the owner/agent fails to delete the equipment and, there is no evidence of movements reported to the TRAIN system, the AAR may delete the equipment.

2. Upon receipt of notice from the owner/agent that their equipment has been sold and will be restencilled with a new reporting mark, the AAR will insert the transportation code 'M' in the records. The owner/agent of the cancelled reporting mark will be provided with the advice of the insertion of the 'M' code. The owner/agent of the cancelled reporting mark will have ninety (90) days, after the insertion of the code 'M', to delete the cars from the UMLER file.

When, after ninety (90) days, the owner/agent of the cancelled reporting marks has not deleted the equipment, or has not requested in writing an extension or extensions having a maximum of thirty (30) days, or there is no evidence of movements reported to the TRAIN system, the AAR may delete the equipment.

III. SUBMISSION OF DATA:

A. Effective Date For Rates: For the purpose of receiving allowances, all data on newly acquired equipment and/or changes to equipment registered in the file which affects the valuation, age or equipment type code (regardless of ownership), must be reported in the month prior to the first day of the month the charges are to become effective.

B. Method: Data can be furnished in document, magnetic tape or telecommunications. The prescribed input formats for documents are shown in the beginning of each section of this manual. Tape input and output formats are covered in Exhibit A. Each UMLER submission must be submitted with the appropriate "paskey" which is assigned by the Director, Train & UMLER Services, Transportation Division. The "paskey" is a four position alphabetic code, known only to the AAR and the equipment owner, which prevents someone other than the equipment owner from submitting data to the file. Documents must have the paskey imprinted on the upper left hand corner of the document; the card image tape paskey record report in positions 1-12: PASSKEY = XXXX positions 13-80 are blank; the tape paskey record report 1-12: PASSKEY = XXXX positions 13-328 are blank.

C. Update of the UMLER Master File: Updates are processed on a daily basis, and all data is batched twice monthly on the 15th and last day of the month. All card, document and tape submissions must be received for processing by 5:00 p.m., E.T., on the 15th or last day of the month. When the 15th or last day of the month falls on a Saturday, Sunday or holiday such submissions must be received by 5:00 p.m., E.T., of the last working day prior to the update.

D. TCAM submissions must be received by 8:00 p.m., E.T. daily and are not affected by Saturdays, Sundays and holidays. The formats for submitting and receiving error messages and/or UMLER updates via telecommunications (TCAM) are indicated in the TRAIN II Users Guide.

SECTIONS

Specifications for Reporting All Cars Except Tank Cars (Formats A & C)
Specifications for Reporting Tank Cars (Format B)
Specifications for Reporting Highway Trailers and Containers (Format D)
Specifications for Reporting Locomotives (Format E)
Specifications and Procedures for the Pool System

Exhibits:

- A. Tape Input and Output Formats and Instructions
- B. Select Field Change Format
- C. Selection Format and Codes
- D. List of AAR Car Type and Trailer and Container Type Codes
- E. Lining Codes
- F. Clearance Plates
- G. Fitting Codes
- H. Codes for Flooring Specifications
- I. Uniform Abbreviated Codes for Licensing States
- J. Ownership Codes
- K. Standard Transmittal Forms

TABLE OF CONTENTS

- L. Procedures for Notification of Error and Valid Transactions
 - M. Major Classes of Tank Cars & AAR, ARA, DOT or ICC Container Specifications
 - N. Procedures for Reporting Other Than Standard TOFC/COFC Unit Initials and Numbers
 - O. Transportation Code Specifications
 - P. Transportation Condition Codes
 - Q. Mileage Rates
 - R. Superstructure Charges
 - S. Procedure for Reporting Flexible Car Hire
 - T. Procedures for Reporting Non-standard Dimensions to the Exception Control File
 - U. UMLER Error Codes used in the TRAIN system
 - V. OT-37 Reporting Procedures
- (Change effective September 1, 1986 shown by symbol *)
(Change effective October 1, 1986 shown by symbol **)
(Change effective January 1, 1987 shown by symbol ***)
(Change effective April 1, 1987 shown by symbol ****)

UMLR SPECIFICATION MANUAL—SECTION I

SECTION I
SPECIFICATIONS AND APPLICABLE FORMATS AND LINE NUMBERS

TYPE: ALL CARS (EXCEPT TANK) FLAT

FORMAT: A C

LINES: 1-5 1-5

LINE COLUMN NO. FIELD DESCRIPTION CAR TYPE

•• NOTE1: Columns 2 through 18 are repeated on Lines 2 through 5.
NOTE2: An asterisk (*) in the Car Type heading indicates required for caboose, maintenance of way cars and locomotives.

1-5	1	Line Number	All *
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Must be 1, 2, 3, 4 or 5
NOTE: Line 5 is applicable only to railroad owned/leased cars rehabilitated under Circular OT-37 Series.

1-5	2	Transaction Code	All *
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To DELETE a car or car series from the file, the transaction code must be according to Exhibit C. There are three methods that can be used: 1) submit the entire first line of information; 2) the complete tape record; or 3) submit UMLR Deletion Format (Exhibit C), e.g., (a.) for a single car submit the appropriate delete code, car initials, low number (high number may be left blank), and applicable AAR car type code or four zeros, (b.) for cars in consecutively numbered series do not report the applicable AAR car type code; report the arithmetic difference between the low and high number.

To ADD a car or consecutively numbered car series to the file, the transaction code must be a two (2).

A change in car initials and/or numbers requires a complete ADD registration (transaction code 2) under the new initials and numbers and a DELETE (transaction code A) of the former initials and numbers.

To CHANGE data (other than car initials and number) on the file, the transaction code must be a three (3). One of two methods can be used:

A. Standard Method: Submit code three (3) in each complete line of information being submitted or the complete tape record.

B. Select Field Method: Transaction code must be a four (4) 1. This method can be used to change as many as five data fields for a single car or consecutively numbered car series. 2. On-line (TCAM) submitters can report up to ten (10) fields of information. (See Train Manual, UMLR section V, page 27). The Select Field Format (Exhibit B) must be used.

- 1) Report car initials.
- 2) Report low number and the high number for consecutively numbered car series.
- 3) Report line number in which data change is to be made.
- 4) Report field identity number, i.e., the first column in which data are reported.
- 5) Report data to be changed. See example below:

Report data to be changed: 05 examples																															
LINE	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29		
	BI	FI	CAR/UNIT	LOW	HIGH	(FIELD)																									
	AI	OI	INITIAL	NUMBER	NUMBER	NO. 3																									
	NI	RI				DATA																									
	FI	FI				CHANGE																									
						NO. 4																									
						FLEXIBLE																									
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The illustration changed the following:

1. Nominal Capacity—Line 3, Cols. 24-26.
2. Outside Length—Line 2, Cols. 20-24.
3. Data Removed—Line 4, Col. 39 (Lining).
4. Transportation Code—Line 4, Col. 19.

NOTE 1: To remove data from a field, leave the data change field blank.

NOTE 2: When reporting numeric fields leading zeroes must be reported.

1-5	3-6	Car Initials	All *
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Report the alphabetic initials or marks stenciled on car. Do not report special characters or punctuation, e.g., "C&NW" report as CNW.

1-5	7-12	Low Car Number	All *
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Lowest car number of a consecutively numbered group of identical cars.
NOTE: For reporting articulated car see Line 4, Col. 35
EXAMPLE: Car number is 123, report 000123.

1-5	13-18	High Car Number	All *
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Report high number of group above, e.g., car number 125 report as 000125. If this field is left blank, it will be assumed equal to the low number. NOTE: No more than 1500 cars can be reported in a series.

1	19-22	AAR Car Type Code	All *
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Four (4) position alphanumeric code giving a general physical description of the type of car. (See Exhibit D for the appropriate code.)

1	23-26	Owner's Mark	All *
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Report either the uniform alphabetic code of the railroad owning the car, or the reporting mark of the private car company owning the car. DO NOT SHOW SPACE BETWEEN TRUST HOLDERS.

This field must be completed. Failure to provide the appropriate owner's mark results in rejection of the transaction.

NOTE: This field is used to count car ownership for Car Service reports and to process car location tracing through TRAIN inquiries. This field must be kept accurate.

1	27-30	Railroad Lessee's Mark	All *
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Report the uniform alphabetic code of railroad. This field may be left blank. If reported, the reporting mark cannot be equal to the owner's field or be a same as reported.

1	31-32	Month Built	All *
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Numeric, report the month the car was built or rebuilt, e.g., 01 = January. This field is mandatory.

EXCEPTIONS: If a railroad-owned car and placed into service secondhand prior to January 1, 1973, report the month placed into service.

MINIMUM—01

1	33-34	Year Built New	All *
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Last two (2) digits of year built, e.g., year built 1981, report 81 in this field. This field never be blank.

NOTE: Cars having car type codes M and N that were built prior to 1981.

1	35-36	Year Rebuilt	All *
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Last two (2) digits of year rebuilt or report zeros if not applicable. Cannot be reported as zero.

1. Railroad cars applicable only to cars meeting status as provided in ICC Accounting Rules, and the AAR Mechanical Interchange Rule 88, Office Manual.
2. Private cars applicable only to cars meeting status as provided in ICC Accounting Rules, Rule 4, Item 195 and AAR Mechanical Interchange Rule 88, Office Manual and Sections A and B, Field Manual.

NOTE: Do not report rebuilt year unless car has been approved by Secretary, Department of Transportation, AAR.

1	37-38	Year 2nd Hand/RR ICC Rebuilt	All *
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Last two (2) digits of year acquired (denotes change in ownership). Year built must be reported if applicable, must also be reported.

1. All Second Hand acquisitions being added to the file must be accompanied by applicable AAR Certification Forms prescribed in AAR Mechanical Interchange Rule 88, Office Manual and appropriate Circular OT-37 form. The OT-37 form is applicable to railroad reporting marks.
2. Railroad cars built prior to January 1, 1973—applicable only to cars meeting status as provided in ICC Accounting Rules but not meeting rebuilt standards prescribed in ICC Accounting Rules.

NOTE: TIME CHARGES WILL BE CALCULATED FROM LATEST YEAR FOR ALL CARS. THE EXCEPTION OF RAILROAD-OWNED CARS WITH SECOND HAND YEAR NEW OF 1973 OR LATER, THEN RATES WILL BE CALCULATED FROM LATEST YEAR NEW OF 1973.

1	39-42	Advanced Installation Date (Mo.-Yr.)	All *
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Month and year car is to be installed into service. Report month in Cols. 39-42, e.g., 41-42, e.g., October 1983 report 1083. Do not report the current month and year of installation date. If left blank or date less than current is provided, the current date will be inserted by the AAR. This field is relative to the reported estimated tare weight.

see Line 3, Col. 31-34.

Cars may be advance registered up to one (1) year, e.g., in January 1983 the advance registration date will be 0184. Question: Do expired installation dates have advanced-Yes! Question: If a car is placed into interchange before the advance registration date do I have to submit an update-Yes! Question: "Is the effective date of advance installation date in the output tape?" Answer—The advance installation date.

1	43-48	Original Cost (\$)	All *
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APPLICABLE TO ALL RAILROAD MARKED CARS, ALSO, PRIVATELY-OWNED COVERED HOPPER (LO) CARS.

1. For railroad-owned cars, report in U.S. dollars the original ledger value of the acquisition cost for cars purchased prior to January 1, 1973. For cars rebuilt prior to January 1, 1973, report the cost as prescribed in the ICC Accounting Rules. For cars acquired January 1, 1973, report the cost as prescribed in AAR Interchange Rule 88, Office Manual and betterments, refrigeration unit and auto-rack costs, unless the acquisition cost was originally capitalized as an integral part of the car.

EXCEPTIONS:

- A. For cars acquired secondhand prior to January 1, 1973, the Original Cost or the acquisition cost may be greater than the current owner's ledger value.
- B. For cars to which retirement of car's components, e.g., running boards, etc., are removed; the ledger value is reduced and therefore may be less than the Original Cost, which is never altered.

2. For privately-owned covered hopper (LO) cars, report in U.S. dollars the original market value of the base car, as provided in Tariff 6007—Series. For all other cars, this field may be blank reported or reported as zero.

NOTE: Raise all cents to the next dollar, e.g., \$5,501.02 = 005502.

MINIMUM—000100

1	49	Indicator for Positive/Negative A&B	All *
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This field must be reported when reporting additions or betterments Line 4, Col. 35, otherwise report blank.

UMLER SPECIFICATION MANUAL—SECTION I—CONTINUED

the addition of a capitalized cost of additions or betterments made to a car not its installation into service.
the subtraction of a capitalized cost of additions or betterments that were from the car subsequent to its installation into service.

50-54	Additions & Betterments Tariff Additions (\$)	(RR) All (Pvt.) Gov. Hop.
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0 RAILROAD MARKED CARS. ALSO, APPLICABLE TO PRIVATELY-OWNED PER (LO) CARS.

id-owned cars report in U.S. dollars the sum of the costs for As and Bs; ns are new parts applied subsequent to the date the car was built and are car- the capital investment account.
ns are improvements of parts of existing equipment through the substituti- superior parts for inferior parts replaced.
port additions and/or betterment costs applied at the time of OT-37 rehabilita- use costs will be added to the ledger following the completion of the car's 30th

ely-owned covered hopper (LO) cars, report in U.S. dollars (if not already in- the original value, Cols. 43-48) the cost of the original into-service freight, d linings, capitalized additions and betterments as authorized by Tariff ries. If there are no applicable tariff additives, this field should be reported as i field is not applicable to other types of privately-owned freight cars.

I cents to the next dollar, e.g., \$5,501.02 = 05502

55-60	Ledger Value (\$) Adjusted Value (\$)	(RR) All (Pvt.) Gov. Hop.
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USED TO CALCULATE CAR HIRE & MILEAGE CHARGES. APPLICABLE TO RKED CARS. ALSO, APPLICABLE TO PRIVATELY-OWNED COVERED HOPPER

-owned cars report in U.S. dollars the original cost, plus additions and bet- ot report cost of refrigerator unit. Do not report cost of auto-rack unless original- s an integral part of the car.
or cars covered under OT-37 Series report original cost plus addition and better- or and subsequent to rehabilitation. Do not include addition and betterment red at the time of rehabilitation.

joined secondhand prior to January 1, 1973 report the original cost or the ac- and subsequent addition and betterment costs, and for cars purchased subse- nber 31, 1972, report the original cost of the original owner and addition and its.

-owned covered hopper (LO) cars, report in U.S. dollars the total of the original i-48) plus the tariff additions (Cols. 49-54). When there are no tariff additions, e as original value in Cols. 43-48.

I cents to the next dollar, e.g., \$5,501.02 = 05502.

61	Indicator	All *
----	-----------	-------

Alpha/Numeric Field:

error with zero rate (Inserted by AAR)

registered with an estimated value and subject to mileage allowances per

je Tariff PHJ 6007-Series.

subject to mileage allowances, per Tariff PHJ-6007-Series.

bject to car hire (time/mileage) charges

bject to car owner's designated rate

bject to reduced car hire rate in accordance with Ex Parte No. 334, Sub. 4.

xhibit S)

subject to special lease arrangement with Car Hire or mileage allowance dif- from normal basis, or has a transportation code S, X, or Y. (Zero rates shown R output.)

id car subject to car hire (time/mileage) being reported with an estimated (required for AAR auditing purposes.)

bject to car hire (time/mileage) charges under the provisions of Circular

' Series having estimated costs. Not applicable to rebuilt cars.

bject to car hire (time/mileage) under the provisions of Circular OT-37 Series.

subject to car hire (time/mileage) under the provisions of Circular OT-37

having estimated addition/betterment costs reported subsequent to car's

itation.

THE INDICATOR HAS BEEN CHANGED TO ZERO, THE CAR OWNER MUST IE APPROPRIATE INDICATOR CODE.

62-80	Blank	All
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must be a 2, Cols. 2-18 must equal Line 1.

19	Clearance	All *
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je to denote plate clearance of car. See Exhibit F.

learance does not exceed Plate B.

learance is greater than Plate B, but does not exceed Plate C.

learance is greater than Plates B and C, but does not exceed Plate E.

learance is greater than Plates B, C and E, but does not exceed Plate F.

learance exceeds Plates B, C, E and F.

must agree with similar stenciling on side of car. G must agree with stenciling on it exceeds Plate F.

20-24	Outside Dimensions—Length	All
-------	---------------------------	-----

istance over pulling faces of couplers in normal positions. For articulated cars i length of the consist. *-Must be between 2 and 16 feet greater than inside Cols. 20-22, inches in Cols. 23-24. Round fraction to the higher inch, e.g., 05

MINIMUM—030 00	MAXIMUM—098 11
MINIMUM—025 00	MAXIMUM—095 00

f car is a GT ore jenny (AAR car type codes K_B_)

MINIMUM—024 00	MAXIMUM—051 11
----------------	----------------

MINIMUM—020 00	MAXIMUM—080 11
----------------	----------------

f car is an HMA ore jenny, (AAR car type codes J_00)

MINIMUM—020 00	MAXIMUM—051 11
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Stock	MINIMUM—038 00	MAXIMUM—096 11
Refrigerator	MINIMUM—030 00	MAXIMUM—098 11
Flat (Including Q_1_)	MINIMUM—024 00	MAXIMUM—124 11
Flat (Articulated)	MINIMUM—024 00	MAXIMUM—466 00 **

*** - NOTE 1: Not applicable to car type code Q_ except, Q_1_.

*** - NOTE 2: Units in excess of 1,000 feet, report 999.11.

2	25-28	Outside Dimensions—Extreme Width	All *
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Numeric, measurement of extreme outside width of car including attachments projecting to greatest extent. Must (1) exceed inside width and (2) agree relationally with height of the extreme width above rail for clearance code reported (See Exhibit F to cross check these two fields). Feet in Cols. 25-26, inches in Cols. 27-28. Round fraction to the higher inch, e.g., 05 1/4" = 06.

If clearance is B	MINIMUM—07 00	MAXIMUM—10 10
If clearance is C	MINIMUM—07 00	MAXIMUM—10 08
If clearance is E	MINIMUM—07 00	MAXIMUM—10 08
If clearance is F	MINIMUM—07 00	MAXIMUM—10 08
If clearance exceeds Plates B,C,E,F (Code G)	MINIMUM—07 00	MAXIMUM—11 11

EXCEPTIONS: Maximum for flats 12 07 and refrigerators 11 02 with Code G.

2	29-32	Outside Dimensions—Hgt. of Ext. Width	All *
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Numeric, measurement is from top of rail to the highest point where the extreme width occurs. Feet in Cols. 29-30, inches in Cols. 31-32. Round fraction to the higher inch, e.g., 05 1/4" = 06.

If clearance is B	MINIMUM—01 00	MAXIMUM—15 01
If clearance is C	MINIMUM—01 00	MAXIMUM—15 06
If clearance is E	MINIMUM—01 00	MAXIMUM—15 09
If clearance is F	MINIMUM—01 00	MAXIMUM—17 00
If clearance exceeds Plates B,C,E,F (Code G)	MINIMUM—01 00	MAXIMUM—18 01

EXCEPTIONS: Maximum for flats 20 00 and refrigerator 17 11 with Code G.

2	33-36	Outside Dimensions—Extreme Height	All *
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Numeric, height from top of rail to extreme projecting height. Feet in Cols. 33-34, inches in Cols. 35-36. Round fraction to the higher inch, e.g., 05 1/4" = 06.

If clearance is B	MINIMUM—02 00	MAXIMUM—15 01
If clearance is C	MINIMUM—02 00	MAXIMUM—15 06
If clearance is E	MINIMUM—02 00	MAXIMUM—15 09
If clearance is F	MINIMUM—02 00	MAXIMUM—17 00
If clearance exceeds Plates B,C,E,F (Code G)	MINIMUM—02 00	MAXIMUM—18 01

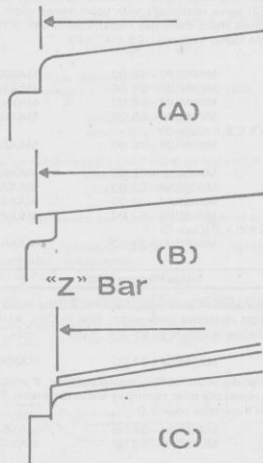
EXCEPTIONS: Maximum for flats 20 00 and refrigerator 17 11 with Code G.

NOTE: The minimum parameter for all cars having Car Type Code V_ is 12 07.

2	37-40	Outside Dimensions—Upper Eaves Width	All except Flat
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Numeric, measurement over top of eaves at side of car. (See diagrams A & B)

EXCEPTION: When cars are equipped with latitudinal running boards, measure from outside edge of top of latitudinal running board to corresponding point on other side. (See diagram C)



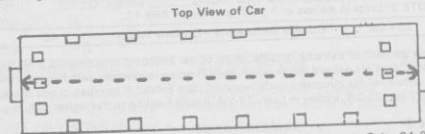
MUST: (1) not exceed the outside extreme width, (2) not be greater than lower eaves width if lower eaves width is reported, (3) agree relationally with height from rail to upper eaves for clearance code reported (See Exhibit F to cross check these two fields). Feet in Cols. 37-38, inches in Cols. 39-40. Round fraction to the higher inch, e.g., 05 1/4" = 06.

If clearance is B	MINIMUM—04 00	MAXIMUM—10 10
If clearance is C	MINIMUM—04 00	MAXIMUM—10 08
If clearance is E	MINIMUM—04 00	MAXIMUM—10 08
If clearance is F	MINIMUM—04 00	MAXIMUM—10 08
If clearance exceeds Plates B,C,E,F (Code G)	MINIMUM—04 00	MAXIMUM—10 11

UMLER SPECIFICATION MANUAL—SECTION I—CONTINUED

2	37-40	Inside Platform Length	Flat
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Numeric, measurement of the length of the loading platform. For multi-level (FA) flat cars report the length of the shortest deck. Feet in Cols. 37-38, inches in Cols. 39-40. See diagram below:



EXCEPTION: If car is equipped with bulkheads (BD or BS in Line 4, Cols. 24-29), measure between bulkheads. Round fraction to the lower inch, e.g., 05 3/4" = 05.

MINIMUM—20 00, ("V" types—MINIMUM 75 00) MAXIMUM—99 04

This data must agree with AAR car type code reported if car type code is any of the following: F_1, L_0, L_2, V_1, and is not applicable to Q_.

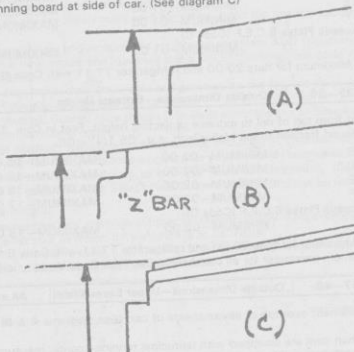
** Numeric, measurement of the length of the loading platform. For car type code Q_ report the most restrictive length dimension of a single platform for the consist.

Flat MINIMUM—20 00 (V = 75 00) MAXIMUM—99 04

This data must agree with the AAR car type code reported if car type code is any of the following: F_1, L_0, L_2, Q_ and V_.

2 41-44 Upper Eaves—Height All except Flat

Numeric, measurement is from rail to top of eaves at side of car (See diagrams A & B). EXCEPTION: When cars are equipped with latitudinal running boards, measure from rail to top of latitudinal running board at side of car. (See diagram C)



MUST: (1) not exceed the extreme height, (2) not be less than the lower eaves height, if lower eaves height is reported, (3) agree relationally with upper eaves width for clearance code reported (See Exhibit F to cross check these two fields). Feet in Cols. 41-42, inches in Cols. 43-44. Round fraction to the higher inch, e.g., 05 1/4" = 06.

Box, Stock & Refrigerator
If clearance is B MINIMUM—08 00 MAXIMUM—15 01
If clearance is C MINIMUM—08 00 MAXIMUM—15 06
If clearance is E MINIMUM—08 00 MAXIMUM—15 09
If clearance is F MINIMUM—08 00 MAXIMUM—17 00
If clearance exceeds Plates B,C,E,F (Code G) MINIMUM—08 00 MAXIMUM—17 11

Gondola & Hopper
If clearance is B MINIMUM—02 00 MAXIMUM—15 01
If clearance is C MINIMUM—02 00 MAXIMUM—15 06
If clearance is E MINIMUM—02 00 MAXIMUM—15 09
If clearance is F MINIMUM—02 00 MAXIMUM—17 00
If clearance exceeds Plates B,C,E,F (Code G) MINIMUM—02 00 MAXIMUM—17 11

2	41-44	Inside Platform Width	Flat
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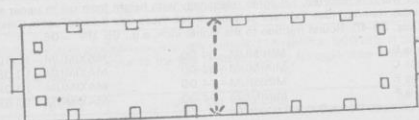
Numeric, measurement of the width of the loading platform. For the inside width of multi-level (FA) flat cars report the most restrictive deck width. Feet in Cols. 41-42, inches in Cols. 43-44. Round fraction to the lower inch, e.g., 05 3/4" = 05.

MINIMUM—04 00 MAXIMUM—12 06

** Numeric, measurement of the width of the loading platform. If articulated and the platforms are different widths, report the most restrictive width dimension. This data must have AAR car type code reported if car type code is Q_.

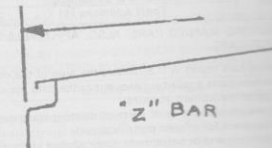
Flat MINIMUM—04 00 MAXIMUM—12 06
Flat (Car Type Code Q) MINIMUM—08 00 MAXIMUM—10 06

Top View of Car



2	45-48	Outside Dimensions—Lower Eaves Width	All except Flat
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Measurement is from outside edge of lower eaves to the corresponding point on opposite side. This measurement is only to be reported if car has "vertical Z bar side plate" construction. (See diagram below)—Otherwise leave zero fill.



Numeric, if not zeros, must: (1) be equal to or greater than upper eaves width, (2) be equal to or less than extreme width, (3) agree relationally with lower eaves height for clearance code reported (See Exhibit F to cross check these two fields). Feet in Cols. 45-46, inches in Cols. 47-48. Round fraction to the higher inch, e.g., 05 1/4" = 06.

Box, Stock & Refrigerator
If clearance is B MINIMUM—07 00 MAXIMUM—15 01
If clearance is C MINIMUM—07 00 MAXIMUM—15 06
If clearance is E MINIMUM—07 00 MAXIMUM—15 09
If clearance is F MINIMUM—07 00 MAXIMUM—17 00
If clearance exceeds Plates B,C,E,F (Code G) MINIMUM—07 00 MAXIMUM—17 11

Gondola & Hopper—May be reported for (1) covered hoppers or (2) gondolas and hoppers with roofs or covers. If reported, parameters are:

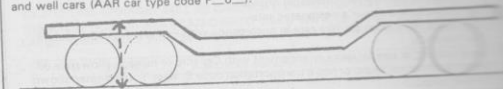
If clearance is B MINIMUM—07 00 MAXIMUM—15 01
If clearance is C MINIMUM—07 00 MAXIMUM—15 06
If clearance is E MINIMUM—07 00 MAXIMUM—15 09
If clearance is F MINIMUM—07 00 MAXIMUM—17 00
If clearance exceeds Plates B,C,E,F (Code G) MINIMUM—07 00 MAXIMUM—17 11

2	45-48	Outside Dimensions—Flat, Hgt. Above Rail	All
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** Numeric, measurement from top of rail to top of loading platform. If articulated and platforms are different widths, report the most restrictive width dimension. EXCEPTIONS: For bi-level and tri-level flat cars, measurement is from top of rail to top of lower deck. Feet in Cols. 45-46, inches in Cols. 47-48. Round fraction to the higher inch, e.g., 05 1/4" = 06. This field must agree relationally for V_1, or top code.

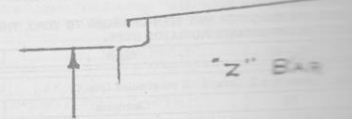
F_1, Q_ MINIMUM—02 00 MAXIMUM—15 01
V_1, V_2, V_3, V_4 MINIMUM—02 00 MAXIMUM—15 06
V_5, V_6 MINIMUM—02 10 MAXIMUM—15 09
V_7, V_8, V_9 MINIMUM—03 04 MAXIMUM—17 00
V_0 MINIMUM—02 00 MAXIMUM—17 11
All except F_3 MINIMUM—02 00 MAXIMUM—17 11
and F_6
All F_3 and F_6 MINIMUM—02 00 MAXIMUM—17 11

See diagram below for place of measurement on depressed cars (AAR car type code F_6_).



2	49-52	Outside Dimensions—Lower Eaves Hgt.	All except Flat
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Numeric, measurement is from rail to top of lower eaves. This measurement is only to be reported for cars with "vertical Z bar" or similar construction. (See diagram below)—Otherwise leave zero fill.



MUST: (1) be less than upper eaves height, (2) be less than extreme height, (3) agree relationally with lower eaves width for clearance code reported (See Exhibit F to cross check these two fields). Feet in Cols. 49-50, inches in Cols. 51-52. Round fraction to the higher inch, e.g., 05 1/4" = 06.

Box, Stock & Refrigerator
If clearance is B MINIMUM—08 00 MAXIMUM—15 01
If clearance is C MINIMUM—08 00 MAXIMUM—15 06
If clearance is E MINIMUM—08 00 MAXIMUM—15 09
If clearance is F MINIMUM—08 00 MAXIMUM—17 00
If clearance exceeds Plates B,C,E,F (Code G) MINIMUM—08 00 MAXIMUM—17 11

Gondola & Hopper—May be reported for (1) covered hoppers or (2) gondolas and hoppers with roofs or covers. If reported, parameters are:

If clearance is B MINIMUM—08 00 MAXIMUM—15 01
If clearance is C MINIMUM—08 00 MAXIMUM—15 06
If clearance is E MINIMUM—08 00 MAXIMUM—15 09
If clearance is F MINIMUM—08 00 MAXIMUM—17 00
If clearance exceeds Plates B,C,E,F (Code G) MINIMUM—08 00 MAXIMUM—17 11

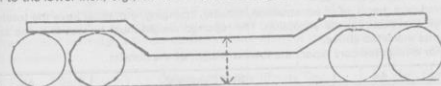
UMLER SPECIFICATION MANUAL—SECTION I—CONTINUED

2	49-52	Bulkhead or Container—Top Width	Flat
		Numeric, width of side door openings. If more than one opening on the side of car, report the width of the maximum continuous opening. Feet in Cols. 53-54, inches in Cols. 55-56. Round fraction to the lower inch, e.g., 05 3/4"=05. If no openings, or permanently closed doors or doors, leave zero fill and report "15" in the Door Type Code. (Cols. 44-45, Line 4).	
		MINIMUM—06 00	MAXIMUM—10 10
		MINIMUM—06 00	MAXIMUM—10 08
		MINIMUM—06 00	MAXIMUM—10 08
		MINIMUM—06 00	MAXIMUM—10 08
		MINIMUM—06 00	MAXIMUM—11 07

2	53-56	Side Door Width	Box, Gon., Stk., Refrig.
		Numeric, width of side door openings. If more than one opening on the side of car, report the width of the maximum continuous opening. Feet in Cols. 53-54, inches in Cols. 55-56. Round fraction to the lower inch, e.g., 05 3/4"=05. If no openings, or permanently closed doors or doors, leave zero fill and report "15" in the Door Type Code. (Cols. 44-45, Line 4).	
		MINIMUM—04 00	MAXIMUM—30 11
		MINIMUM—04 00	MAXIMUM—50 11
		Blank unless car equipped with side doors, then:	
		MINIMUM—01 00	MAXIMUM—60 11
		MINIMUM—04 00	MAXIMUM—30 11
		NOTE: For double and triple deck stock cars (AAR car type codes S_2_, S_7_), report width of smallest opening.	
		MINIMUM—03 00	MAXIMUM—21 11

2	53-56	Bulkhead or Container—Hgt. Above Plat.	Flat
		Numeric, height of the operating end door opening in the clear. Report feet in Cols. 65-66, inches in Cols. 67-68 or zeroes if no end doors. Round fraction to the lower inch, e.g., 05 3/4"=05.	
		MINIMUM—03 00	MAXIMUM—16 03

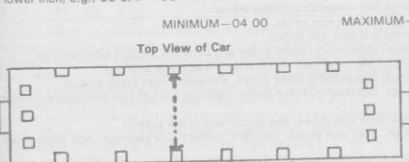
Depressed or Well—Height of Floor
Numeric, measurement (1) inside height of permanent container (CN) in Line 4, Cols. 33-34 or (2) height from loading platform to top of bulkhead if car equipped with bulkheads car type codes F_4_, F_5_, LO2_, LO3_, P_ and Q_. If articulated and the bulkheads have different height dimensions, report the greatest height dimension. Feet in Cols. 53-54, inches in Cols. 55-56. Round fraction to the lower inch, e.g., 05 3/4"=05.



MINIMUM—00 06 MAXIMUM—05 11

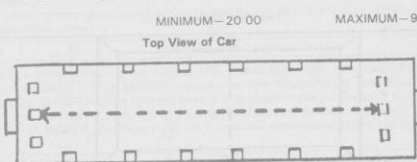
2	57-60	Side Door Height	Box, Gon., Stk., Refrig.
		Numeric, measurement is height of side door opening in the clear. Zero if Line 2, Cols. 53-56 blank. Feet in Cols. 57-58, inches in Cols. 59-60. Round fraction to the lower inch, e.g., 05 3/4"=05. If no opening or permanently closed and/or no side door leave blank and report "15" in the Door Type Code Line 4, Cols. 44-45.	
		MINIMUM—03 00	MAXIMUM—12 11
		MINIMUM—01 00	MAXIMUM—12 11

2	57-60	Inset Stake Pockets—Platform Width	Flat
		Numeric, measurement is between the inside of the side stake pockets. Feet in Cols. 57-58, inches in Cols. 59-60. Round fraction to the lower inch, e.g., 05 3/4"=05.	
		MINIMUM—04 00	MAXIMUM—11 06



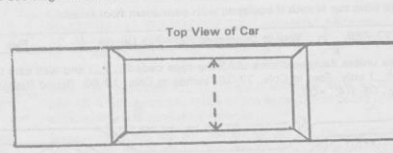
2	61-64	End Door Width	Box, Gon., Stk.
		Numeric, the width of the operating end door opening in the clear. Feet in Cols. 61-62, inches in Cols. 63-64. Round fraction to the lower inch, e.g., 05 3/4"=05. If no such door or end door is inoperable, this field must be zero filled.	
		MINIMUM—01 00	MAXIMUM—11 11
		Zero, unless Drop End Gondolas car type codes G_2_, G_4_, E_0_ or J_0_.	
		MINIMUM—05 05	MAXIMUM—11 11
		MINIMUM—01 00	MAXIMUM—11 11

2	61-64	Inset Stake Pockets—Platform Length	Flat
		Numeric, report zeroes unless car has inset stake pockets. Then the measurement is between the insides of the end stake pockets. Feet in Cols. 61-62, inches in Cols. 63-64. Round fraction to the lower inch, e.g., 05 3/4"=05.	
		MINIMUM—20 00	MAXIMUM—99 11



2	65-68	End Door Height	Box, Gon., Stk.
		Numeric, height of the operating end door opening in the clear. Report feet in Cols. 65-66, inches in Cols. 67-68 or zeroes if no end doors. Round fraction to the lower inch, e.g., 05 3/4"=05.	
		MINIMUM—00 06	MAXIMUM—11 11
		Zero, unless Drop End Gondolas car type codes G_2_, G_4_, E_0_, J_0_.	
		MINIMUM—01 00	MAXIMUM—10 11
		MINIMUM—00 06	MAXIMUM—12 11

2	65-68	Well or Depressed Flat—Bottom Width	Flat
		Numeric, zero unless depressed car (AAR car type code F_3_) or well car (AAR car type code F_6_). Feet in Cols. 65-66, inches in Cols. 67-68. Round fraction to the lower inch, e.g., 05 3/4"=05. See diagram below.	



2	69-72	Inside Dimensions—Length	All except Flat
		Numeric, measurement to be taken from end to end between linings. Must be less than outside length. Feet in Cols. 69-70, inches in Cols. 71-72. Round fraction to the lower inch, e.g., 05 3/4"=05.	
		MINIMUM—25 00	MAXIMUM—93 11

Box
NOTE: Must agree with AAR car type code (Exceptions: L_4_ and L_7_).

Gondola
MINIMUM—19 00 MAXIMUM—85 00

NOTE: Must agree with AAR car type code.

EXCEPTION: If car is a GT ore jenny (AAR car type code J_0_0).

Hopper
MINIMUM—19 00 MAXIMUM—35 11
MINIMUM—17 00 MAXIMUM—75 11

EXCEPTION: If car is an HMA ore jenny (AAR car type code K_8_).

Stock
MINIMUM—17 00 MAXIMUM—35 11
MINIMUM—35 00 MAXIMUM—90 11

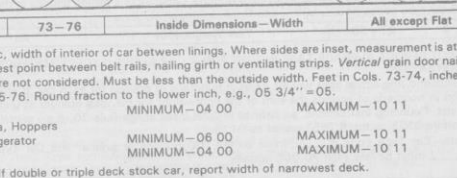
NOTE: If double or triple deck stock car, report inside length of shortest deck.

Refrigerator
MINIMUM—25 00 MAXIMUM—93 11

NOTE: Must agree with AAR car type code.

NOTE: Not applicable to R_8_.

2	69-72	Well or Depressed Flat—Bottom Length	Flat
		Numeric, zeroes unless depressed car (AAR car type code F_3_) or well car (AAR car type code F_6_). Feet in Cols. 69-70, inches in Cols. 71-72. Round fraction to the lower inch, e.g., 05 3/4"=05.	
		MINIMUM—07 06	MAXIMUM—56 11

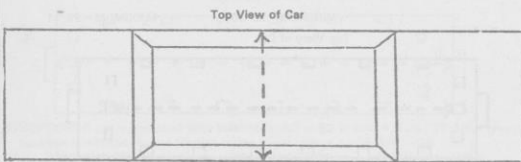


NOTE: If double or triple deck stock car, report width of narrowest deck.

UMLER SPECIFICATION MANUAL—SECTION I—CONTINUED

2	73-76	Well or Depressed Flat—Top Width	Flat
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Numeric, zeroes unless depressed cars (AAR car type code F_3_) and well cars (AAR car type code F_6_) only. Feet in Cols. 73-74, inches in Cols. 75-76. Round fraction to the lower inch, e.g., 05 3/4" = 05.



	MINIMUM—03 08	MAXIMUM—11 11
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2	77-80	Inside Dimension—Height	All ex. Hop. & Flat
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Numeric, measurement is taken from floor to carline or to lowest point of roof, whichever is lower. Must be less than extreme height. Feet in Cols. 77-78, inches in Cols. 79-80. Round fraction to the lower inch, e.g., 05 3/4" = 05.

Box	MINIMUM—05 05	MAXIMUM—15 11
Gondola	Measurement must be taken from floor to top of side.	
Hopper	Report zeroes	MINIMUM—01 00
Stock	MINIMUM—03 00	MAXIMUM—15 11

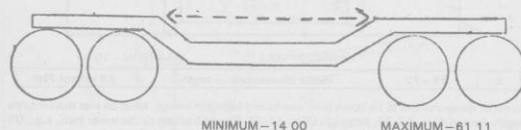
NOTE: If double or triple deck stock car, report smallest inside height.

Refrigerator	MINIMUM—06 00	MAXIMUM—15 11
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NOTE: Measure from top of rack if equipped with permanent floor racks.

2	77-80	Well or Depressed Flat—Top Length	Flat
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Numeric, zeroes unless depressed cars (AAR car type code F_3_) and well cars (AAR car type code F_6_) only. Feet in Cols. 77-78, inches in Cols. 79-80. Round fraction to the lower inch, e.g., 05 1/4" = 05.



	MINIMUM—14 00	MAXIMUM—61 11
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Line 3, Col. 1 must be a 3, Cols. 2-18 must be equal to Lines 1 and 2.

3	19-23	Cubic Feet Capacity—Actual	All
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Numeric actual, i.e., drawing dimensions, inside volume of car in cubic feet—end to end, side to side, and from floor to carline.

Box	MINIMUM—02000	MAXIMUM—11000
NOTE: If automobile box cars equipped with loading racks and can be loaded with other commodities, report a cubic capacity reduced by the amount corresponding to the over-all depth of the racks when raised in a stored position against roof of car.		
Gondola & Hopper	MINIMUM—00400	MAXIMUM—08500

NOTE: For covered hoppers this field must agree with AAR car type code.

Stock	MINIMUM—01000	MAXIMUM—10000
Refrigerator	MINIMUM—01400	MAXIMUM—05700
Flat fixed container (Fitting code CN)	MINIMUM—06500	MAXIMUM—09500

3	24-26	Nominal Capacity (000)	All
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** Numeric, capacity in thousands of pounds as stenciled on car and defined in AAR Interchange Rule 70. If articulated (car type code Q_) report in thousands of pounds the total capacity of the consist.

Box	MINIMUM—035	MAXIMUM—200
Gondola	MINIMUM—050	MAXIMUM—300
Hopper	MINIMUM—060	MAXIMUM—300
Stock	MINIMUM—020	MAXIMUM—200
Refrigerator	MINIMUM—050	MAXIMUM—200
Flat (Including Q_1_)	MINIMUM—040	MAXIMUM—800
Flat (Articulated)	MINIMUM—040	MAXIMUM—999

For AAR car type codes F1_, F2_, F3_, F4_, F5_, this field must agree with the AAR car type code reported.

EXCEPTION: Report 999 for cars having a Nominal Capacity of a million pounds or more with AAR car type codes F43_, L09_ and Q_2_ through Q_9_.

3	27-30	Tare Weight (00)	All
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DO NOT REPORT AVERAGE TARE WEIGHT FOR A CAR SERIES (EXCEPT ADVANCE REGISTRATION).

** Numeric, the actual light weight (tare) in hundreds of pounds for each car. If articulated, report in hundreds of pounds the sum of the lightweight for the total number of units of the consist. Rounding instructions, as defined in AAR Interchange Rule 70, e.g., actual 17550 report as 0175; actual 17551 report as 0176.

Note—Car Type code P_ must be 33,100 pounds or greater and Car Type Code Q_ must be less than 70,000 pounds

Box	MINIMUM—0160	MAXIMUM—1000
Gondola	MINIMUM—0300	MAXIMUM—1000
Hopper	MINIMUM—0300	MAXIMUM—1000
Stock	MINIMUM—0300	MAXIMUM—1000
Refrigerator	MINIMUM—0160	MAXIMUM—1000
Flat	MINIMUM—0230	MAXIMUM—1000
Flat (FC) P_	MINIMUM—0331	MAXIMUM—1000
Flat (Q_1_) Single Platform		
form	MINIMUM—0230	MAXIMUM—1000
Flat (FCA) Multi-platform	MINIMUM—0460	MAXIMUM—1000
Auto rack included for all AAR car type codes V_		
	MINIMUM—0700	MAXIMUM—1000

NOTE—When reporting new cars (except advance registration) and cars that have been reweighed the weighing road (Cols. 31-34) and weighing date (Cols. 35-40) must be reported.

3	31-34	Weighing Road	All
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1. Car weighed or reweighed by a railroad, report uniform alphabetic code of weighing road.
2. Car weighed or reweighed by owner, report owner's reporting mark.
3. Car weighed or reweighed by other than owner or railroad, report owner's mark.
4. When reporting new cars or advanced registered cars and the tare weight is not reported weight report EST_. EST will be indicated as an error 60 days after the service date.
5. For new cars, the word "NEW" can be substituted for company mark.

NOTE: When reporting new cars (except advanced registered) and cars that have been reweighed, the tare weight (Cols. 27-30) and the weighing date field (Cols. 35-40) must be reported.

3	35-40	Weighing Date (Mo-Day-Yr)	All
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Report as one (1) complete six (6) position, numeric, date field. Month (35-36), day (37-38), and year (39-40) the car was weighed or reweighed in accordance with AAR Interchange Rule 70.

NOTE 1: When reporting new cars and cars that have been reweighed, the tare weight (Cols. 27-30) and the weighing road (Cols. 31-34) must be reported.

NOTE 2: When reporting new or advance registered cars and the tare weight is not reported weight report zeroes.

3	41-43	Total Allowable Weight on Rail (000)	All
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Numeric, the actual total weight allowable on rail based on journal size (AAR Manual Standards & Recommended Practices), wheel size or car construction, wheel trucks, etc. Journal Size 4 1/2 x 8 5/8 5 1/2 x 10 6 x 11 8 1/2 x 12 10 x 14 12 x 16 14 x 18 16 x 20 18 x 22 20 x 24 22 x 26 24 x 28 26 x 30 28 x 32 30 x 34 32 x 36 34 x 38 36 x 40 38 x 42 40 x 44 42 x 46 44 x 48 46 x 50 48 x 52 50 x 54 52 x 56 54 x 58 56 x 60 58 x 62 60 x 64 62 x 66 64 x 68 66 x 70 68 x 72 70 x 74 72 x 76 74 x 78 76 x 80 78 x 82 80 x 84 82 x 86 84 x 88 86 x 90 88 x 92 90 x 94 92 x 96 94 x 98 96 x 100 98 x 102 100 x 104 102 x 106 104 x 108 106 x 110 108 x 112 110 x 114 112 x 116 114 x 118 116 x 120 118 x 122 120 x 124 122 x 126 124 x 128 126 x 130 128 x 132 130 x 134 132 x 136 134 x 138 136 x 140 138 x 142 140 x 144 142 x 146 144 x 148 146 x 150 148 x 152 150 x 154 152 x 156 154 x 158 156 x 160 158 x 162 160 x 164 162 x 166 164 x 168 166 x 170 168 x 172 170 x 174 172 x 176 174 x 178 176 x 180 178 x 182 180 x 184 182 x 186 184 x 188 186 x 190 188 x 192 190 x 194 192 x 196 194 x 198 196 x 200 198 x 202 200 x 204 202 x 206 204 x 208 206 x 210 208 x 212 210 x 214 212 x 216 214 x 218 216 x 220 218 x 222 220 x 224 222 x 226 224 x 228 226 x 230 228 x 232 230 x 234 232 x 236 234 x 238 236 x 240 238 x 242 240 x 244 242 x 246 244 x 248 246 x 250 248 x 252 250 x 254 252 x 256 254 x 258 256 x 260 258 x 262 260 x 264 262 x 266 264 x 268 266 x 270 268 x 272 270 x 274 272 x 276 274 x 278 276 x 280 278 x 282 280 x 284 282 x 286 284 x 288 286 x 290 288 x 292 290 x 294 292 x 296 294 x 298 296 x 300 298 x 302 300 x 304 302 x 306 304 x 308 306 x 310 308 x 312 310 x 314 312 x 316 314 x 318 316 x 320 318 x 322 320 x 324 322 x 326 324 x 328 326 x 330 328 x 332 330 x 334 332 x 336 334 x 338 336 x 340 338 x 342 340 x 344 342 x 346 344 x 348 346 x 350 348 x 352 350 x 354 352 x 356 354 x 358 356 x 360 358 x 362 360 x 364 362 x 366 364 x 368 366 x 370 368 x 372 370 x 374 372 x 376 374 x 378 376 x 380 378 x 382 380 x 384 382 x 386 384 x 388 386 x 390 388 x 392 390 x 394 392 x 396 394 x 398 396 x 400 398 x 402 400 x 404 402 x 406 404 x 408 406 x 410 408 x 412 410 x 414 412 x 416 414 x 418 416 x 420 418 x 422 420 x 424 422 x 426 424 x 428 426 x 430 428 x 432 430 x 434 432 x 436 434 x 438 436 x 440 438 x 442 440 x 444 442 x 446 444 x 448 446 x 450 448 x 452 450 x 454 452 x 456 454 x 458 456 x 460 458 x 462 460 x 464 462 x 466 464 x 468 466 x 470 468 x 472 470 x 474 472 x 476 474 x 478 476 x 480 478 x 482 480 x 484 482 x 486 484 x 488 486 x 490 488 x 492 490 x 494 492 x 496 494 x 498 496 x 500 498 x 502 500 x 504 502 x 506 504 x 508 506 x 510 508 x 512 510 x 514 512 x 516 514 x 518 516 x 520 518 x 522 520 x 524 522 x 526 524 x 528 526 x 530 528 x 532 530 x 534 532 x 536 534 x 538 536 x 540 538 x 542 540 x 544 542 x 546 544 x 548 546 x 550 548 x 552 550 x 554 552 x 556 554 x 558 556 x 560 558 x 562 560 x 564 562 x 566 564 x 568 566 x 570 568 x 572 570 x 574 572 x 576 574 x 578 576 x 580 578 x 582 580 x 584 582 x 586 584 x 588 586 x 590 588 x 592 590 x 594 592 x 596 594 x 598 596 x 600 598 x 602 600 x 604 602 x 606 604 x 608 606 x 610 608 x 612 610 x 614 612 x 616 614 x 618 616 x 620 618 x 622 620 x 624 622 x 626 624 x 628 626 x 630 628 x 632 630 x 634 632 x 636 634 x 638 636 x 640 638 x 642 640 x 644 642 x 646 644 x 648 646 x 650 648 x 652 650 x 654 652 x 656 654 x 658 656 x 660 658 x 662 660 x 664 662 x 666 664 x 668 666 x 670 668 x 672 670 x 674 672 x 676 674 x 678 676 x 680 678 x 682 680 x 684 682 x 686 684 x 688 686 x 690 688 x 692 690 x 694 692 x 696 694 x 698 696 x 700 698 x 702 700 x 704 702 x 706 704 x 708 706 x 710 708 x 712 710 x 714 712 x 716 714 x 718 716 x 720 718 x 722 720 x 724 722 x 726 724 x 728 726 x 730 728 x 732 730 x 734 732 x 736 734 x 738 736 x 740 738 x 742 740 x 744 742 x 746 744 x 748 746 x 750 748 x 752 750 x 754 752 x 756 754 x 758 756 x 760 758 x 762 760 x 764 762 x 766 764 x 768 766 x 770 768 x 772 770 x 774 772 x 776 774 x 778 776 x 780 778 x 782 780 x 784 782 x 786 784 x 788 786 x 790 788 x 792 790 x 794 792 x 796 794 x 798 796 x 800 798 x 802 800 x 804 802 x 806 804 x 808 806 x 810 808 x 812 810 x 814 812 x 816 814 x 818 816 x 820 818 x 822 820 x 824 822 x 826 824 x 828 826 x 830 828 x 832 830 x 834 832 x 836 834 x 838 836 x 840 838 x 842 840 x 844 842 x 846 844 x 848 846 x 850 848 x 852 850 x 854 852 x 856 854 x 858 856 x 860 858 x 862 860 x 864 862 x 866 864 x 868 866 x 870 868 x 872 870 x 874 872 x 876 874 x 878 876 x 880 878 x 882 880 x 884 882 x 886 884 x 888 886 x 890 888 x 892 890 x 894 892 x 896 894 x 898 896 x 900 898 x 902 900 x 904 902 x 906 904 x 908 906 x 910 908 x 912 910 x 914 912 x 916 914 x 918 916 x 920 918 x 922 920 x 924 922 x 926 924 x 928 926 x 930 928 x 932 930 x 934 932 x 936 934 x 938 936 x 940 938 x 942 940 x 944 942 x 946 944 x 948 946 x 950 948 x 952 950 x 954 952 x 956 954 x 958 956 x 960 958 x 962 960 x 964 962 x 966 964 x 968 966 x 970 968 x 972 970 x 974 972 x 976 974 x 978 976 x 980 978 x 982 980 x 984 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1154 1152 x 1156 1154 x 1158 1156 x 1160 1158 x 1162 1160 x 1164 1162 x 1166 1164 x 1168 1166 x 1170 1168 x 1172 1170 x 1174 1172 x 1176 1174 x 1178 1176 x 1180 1178 x 1182 1180 x 1184 1182 x 1186 1184 x 1188 1186 x 1190 1188 x 1192 1190 x 1194 1192 x 1196 1194 x 1198 1196 x 1200 1198 x 1202 1200 x 1204 1202 x 1206 1204 x 1208 1206 x 1210 1208 x 1212 1210 x 1214 1212 x 1216 1214 x 1218 1216 x 1220 1218 x 1222 1220 x 1224 1222 x 1226 1224 x 1228 1226 x 1230 1228 x 1232 1230 x 1234 1232 x 1236 1234 x 1238 1236 x 1240 1238 x 1242 1240 x 1244 1242 x 1246 1244 x 1248 1246 x 1250 1248 x 1252 1250 x 1254 1252 x 1256 1254 x 1258 1256 x 1260 1258 x 1262 1260 x 1264 1262 x 1266 1264 x 1268 1266 x 1270 1268 x 1272 1270 x 1274 1272 x 1276 1274 x 1278 1276 x 1280 1278 x 1282 1280 x 1284 1282 x 1286 1284 x 1288 1286 x 1290 1288 x 1292 1290 x 1294 1292 x 1296 1294 x 1298 1296 x 1300 1298 x 1302 1300 x 1304 1302 x 1306 1304 x 1308 1306 x 1310 1308 x 1312 1310 x 1314 1312 x 1316 1314 x 1318 1316 x 1320 1318 x 1322 1320 x 1324 1322 x 1326 1324 x 1328 1326 x 1330 1328 x 1332 1330 x 1334 1332 x 1336 1334 x 1338 1336 x 1340 1338 x 1342 1340 x 1344 1342 x 1346 1344 x 1348 1346 x 1350 1348 x 1352 1350 x 1354 1352 x 1356 1354 x 1358 1356 x 1360 1358 x 1362 1360 x 1364 1362 x 1366 1364 x 1368 1366 x 1370 1368 x 1372 1370 x 1374 1372 x 1376 1374 x 1378 1376 x 1380 1378 x 1382 1380 x 1384 1382 x 1386 1384 x 1388 1386 x 1390 1388 x 1392 1390 x 1394 1392 x 1396 1394 x 1398 1396 x 1400 1398 x 1402 1400 x 1404 1402 x 1406 1404 x 1408 1406 x 1410 1408 x 1412 1410 x 1414 1412 x 1416 1414 x 1418 1416 x 1420 1418 x 1422 1420 x 1424 1422 x 1426 1424 x 1428 1426 x 1430 1428 x 1432 1430 x 1434 1432 x 1436 1434 x 1438 1436 x 1440 1438 x 1442 1440 x 1444 1442 x 1446 1444 x 1448 1446 x 1450 1448 x 1452 1450 x 1454 1452 x 1456 1454 x 1458 1456 x 1460 1458 x 1462 1460 x 1464 1462 x 1466 1464 x 1468 1466 x 1470 1468 x 1472 1470 x 1474 1472 x 1476 1474 x 1478 1476 x 1480 1478 x 1482 1480 x 1484 1482 x 1486 1484 x 1488 1486 x 1490 1488 x 1492 1490 x 1494 1492 x 1496 1494 x 1498 1496 x 1500 1498 x 1502 1500 x 1504 1502 x 1506 1504 x 1508 1506 x 1510 1508 x 1512 1510 x 1514 1512 x 1516 1514 x 1518 1516 x 1520 1518 x 1522 1520 x 1524 1522 x 1526 1524 x 1528 1526 x 1530 1528 x 1532 1530 x 1534 1532 x 1536 1534 x 1538 1536 x 1540 1538 x 1542 1540 x 1544 1542 x 1546 1544 x 1548 1546 x 1550 1548 x 1552 1550 x 1554 1552 x 1556 1554 x 1558 1556 x 1560 1558 x 1562 1560 x 1564 1562 x 1566 1564 x 1568 1566 x 1570 1568 x 1572 1570 x 1574 1572 x 1576 1574 x 1578 1576 x 1580 1578 x 1582 1580 x 1584 1582 x 1586 1584 x 1588 1586 x 1590 1588 x 1592 1590 x 1594 1592 x 1596 1594 x 1598 1596 x 1600 1598 x 1602 1600 x 1604 1602 x 1606 1604 x 1608 1606 x 1610 1608 x 1612 1610 x 1614 1612 x 1616 1614 x 1618 1616 x 1620 1618 x 1622 1620 x 1624 1622 x 1626 1624 x 1628 1626 x 1630 1628 x 1632 1630 x 1634 1632 x 1636 1634 x 1638 1636 x 1640 1638 x 1642 1640 x 1644 1642 x 1646 1644 x 1648 1646 x 1650 1648 x 1652 1650 x 1654 1652 x 1656 1654 x 1658 1656 x 1660 1658 x 1662 1660 x 1664 1662 x 1666 1664 x 1668 1666 x 1670 1668 x 1672 1670 x 1674 1672 x 1676 1674 x 1678 1676 x 1680 1678 x 1682 1680 x 1684 1682 x 1686

UMLER SPECIFICATION MANUAL—SECTION I—CONTINUED

3	50	Axles	All *
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Report the applicable alphanumeric code indicating the number of axles per car.

2 = 2 axles per car,	4 = 4 axles per car,	6 = 6 axles per car,
3 = 3 axles per car,	9 = 9 axles per car,	0 = 10 axles per car
4 = 11 axles per car,	B = 12 axles per car,	C = 13 axles per car
5 = 14 axles per car,	E = 15 axles per car,	F = 16 axles per car
6 = 17 axles per car,	H = 18 axles per car,	I = 19 axles per car
7 = 20 axles per car,	K = 21 axles per car,	L = 22 axles per car
8 = 23 axles per car,	N = 24 axles per car,	O = 25 axles per car
9 = 26 axles per car,	Q = 27 axles per car,	R = 28 axles per car
10 = 29 axles per car,	T = 30 axles per car,	U = 31 axles per car
11 = 32 axles per car,	W = 33 axles per car,	X = 34 axles per car
12 = 35 axles per car,	Z = 36 or more axles per car,	

3	51	Truck Type & Axle Spacing	All
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Alphabetic code indicating truck type and axle spacing with devices to control stability of freight cars in accordance with Rule 88B, Item 9a, Field Manual and Rule 88, Section B, Spring Truck Office Manual, AAR Interchange Rules, and distance of axle spacing per standard two (2) axle truck.

1 = 5 feet 1 inch	D = 5 feet 8 inches	G = Non-standard dimensions
2 = 5 feet 3 inches	E = 5 feet 10 inches	
3 = 5 feet 6 inches	F = 6 feet	

Alphabetic code indicating truck type and axle spacing without devices to control stability of freight cars and distance of axle spacing per standard two (2) axle truck.

1 = 5 feet 1 inch	M = 5 feet 8 inches	Q = Non-standard dimensions
2 = 5 feet 3 inches	N = 5 feet 10 inches	
3 = 5 feet 6 inches	P = 6 feet	

Cars not having standard two (2) axle trucks report a blank.

Articulated cars having standard two (2) axle trucks report the appropriate code.

3	52	Wheel Size	All
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Numeric code indicating the wheel size which is standard to the car according to its designation in Rule 41, Section E, AAR Interchange Rules.

1 = 28 Inch Wheel	4 = 36 Inch Wheel
2 = 30 Inch Wheel	5 = 38 Inch Wheel
3 = 33 Inch Wheel	6 = 33 and 36 Inch Wheels (Articulated Cars Only)

NOTE: To report code 6, the car must be registered as an articulated car, Line 4, Col. 35.

3	53-54	Draft Gear	All
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Standard draft gear report 88. If equipped with one (1) rotary coupler report 89, or, if equipped with two (2) rotary couplers report 96. (Applicable to AAR car type codes G_____, H_____, J_____, K_____, L_____, and C_____.)

Car equipped with standard draft gear with F-bottom shelf (only) couplers report 66, or, if equipped with standard draft gear with E, F or E/F double top and bottom shelf couplers report 77. These codes applicable to AAR car type code C_____. Only.

All others report number of inches of travel in draft gear or center sill of cars with cushioned underframe from normal position to maximum extension for one end of car.

MINIMUM—05	MAXIMUM—36
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3	55-57	Center of Gravity (Empty Car)	Flat, Gond.
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Numeric, report the height from top of rail to the center of gravity for car type codes F_____, G_____, H_____, I_____, J_____, K_____, L_____, M_____, N_____, O_____, P_____, Q_____, R_____, S_____, T_____, U_____, V_____, W_____, X_____, Y_____, and Z_____. Feet in Col. 55 and inches in Cols. 56-57. Round fractions to the higher inch, e.g., 05 3/4" = 06.

MINIMUM—110	MAXIMUM—211
MINIMUM—207	MAXIMUM—411

3	** 58-66	Blank	(Pvt) All
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3	** 58	Blank	(RR) All
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3	59-62	Flexible Car Hire Time Charge	(RR) All
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Report the flexible time charge, must be less than the Ex Parte No. 334 rate and, if applicable, less than the sum of the Ex Parte No. 334 rate and the OT-37 supplemental charge.

3	63-66	Flexible Mileage Charge	(RR) All
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Report the flexible mileage charge, must be less than or equal to the Ex Parte No. 334 mileage charge.

3	67-70	Load Limit (00)	All
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Numeric, report in hundredths of pounds the stencilled load limit on the car. The load limit is the maximum permissible weight that can be loaded on the car. This value is determined by subtracting the car's light weight from the total allowable weight on rail for the applicable journal size as defined in A.A.R. Interchange Rule 70. The value reported can be equal to or greater than the car's nominal capacity however, when added to the tare weight the sum cannot exceed the Total Allowable Weight on Rail. See Line 3, cols. 41-43.

For articulated intermodal flat car the load limit per unit to be stencilled on car, an exception to procedures used on single cars. Method for determining load limit per unit will be to determine lightweight of every unit and subtract this from the axle or wheel capacity, whichever is less, under the specific unit, which must be equal to the lowest value reported for A, B, or C in Line 4, Cols. 41-44, 55-58 or 69-72.

3	71-80	Blank	All
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Line 4, Col. 1 must equal 4. Cols. 2-18 must equal Lines 1, 2, and 3.

4	19	Transportation Code	All *
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Alphabetic code indicating type of assigned service, empty (boxcar) routing or restriction of the car. See Exhibit O for appropriate codes and types of cars permitted in assigned service. An 'X' or 'Y' Code in this field must be accompanied by an appropriate interchange code in Line 4, Col. 20. (See Exhibit P).

Codes E, F, G, S, X or Y can be submitted in UMLER updates.

The code M will be reported by the AAR.

The code O will be reported by the AAR after approval of the request from the owner/lessee.

4. The code T will be generated by the AAR based on movement information reported to the TRAIN System (See Section V, Item M).

5. The code Z will be reported by the AAR and will be accompanied with a condition code 'C' (Line 4, Col. 20) when no movement advices have been reported to the TRAIN System in over 365 days.

6. All other transportation codes are generated by the AAR in the pool system (see Exhibit O for appropriate codes).

4	20	Transportation Condition Code	All
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Alphabetic code describing the AAR or FRA interchange restriction. See Exhibit P for code description. NOTE 1: M may be blank, except when the transportation code Line 4, Col. 19, X, Y or Z are present. Then the field will have the applicable code per Exhibit P or, if the transportation code T is present (Line 4, Col. 19) then the field may be blank or contain Transportation Code E or O. NOTE 2: The removal of the T code will automatically reinstate the E or O in Line 4, Col. 19.

4	21-23	Notes	All
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Report any numeric note references displayed in *The Official Railway Equipment Register*.

NOTE: If up to 3 notes are applicable, and the note references are limited to 1 character each, start in the high order position (Line 4, Col. 21) with the first reference and continue through the other two columns, e.g., AMRX 1100-1112 shows note references 1, 2, 3.

Report: 1 2 3
In Columns: 21 22 23

4	24-29	Fittings (1-2-3)	All except Stock
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Three 2-position codes showing types of equipment installed in the car. May be alphabetic or numeric. See Exhibit G for fitting codes.

Fitting 1— If only one fitting, report in Cols. 24-25.

Fitting 2— If two fittings, report in Cols. 24-25 and 26-27.

Fitting 3— If three fittings, report in Cols. 24-25, 26-27 and 28-29.

Box For plain box (AAR car type code B____), this field may be blank or AB, AD, AP, AS, DA, SB, if numeric minimum-01, maximum-31, do not use 30, HH or HP for B____ or B____ only. For equipped box (AAR car type code A____, L____, L____) may be blank or if numeric, MINIMUM—01, MAXIMUM—59. DO NOT USE 30/ If alphabetic, fields may be AB, AD, AF, AP, AS, BA, BD, BE, BN, BS, CD, CN, DA, DD, HH, IB, PA, PB, PD, PF, RB, RF, RI, RR, SA, SB, SF, SN, TD, HP for A____ only. For A____ box cars, fitting DD does not apply.

Note: Car Type Code A____ car is equipped AB & AS report BA, equipped AS & SB report SA. If one of the combination codes is reported—then a component code cannot be reported.

Refrig. Blank or codes AB, AD, AF, AP, AS, BE, BD, BS, DF, DO, FD, FF, GR, HH, IB, MR, OF, PA, PF, RF, RI, SB, SF, WR, ZR. Code RM applicable to RP, RS, type cars, TM applicable to RPL cars and FS applicable to RPB, RP, RPL cars. If numeric MINIMUM—01, MAXIMUM—59. DO NOT USE 30/ Note: Car equipped AF & BD report DF, equipped AF & BS report SF or equipped BD & FD report DO

Gondola If one jenny J____, these fields must be blank. Other unequipped gondola cars (AAR car type code G____), these fields may be blank or contain AS, BN, BS, CD, RI or DB for J____ only. If equipped gondola (AAR car type code E____), or L016, L017, L018, fields may be blank or contain AS, BD, BE, BN, BS, CD, CN, CV, ST, TD. If numeric MINIMUM—01, MAXIMUM—59. DO NOT USE 30/

Flat Use only AS, BD, BN, BS, CD, CN, CV, RI, ST, TD, or (TH on FC and FCA type cars) or (BC on FB, FBS type cars) or (CB on FB, FC, FMS & FL type cars), or (SD on FB, FBS & FMS type cars), or SC, WS on FA type cars) or (NB on FM, FMS, FB, FBS type cars) or (TL on LF type cars).

Hopper If unequipped hopper (AAR car type code H____), or one jenny J____, these fields must be blank. If equipped hopper (AAR car type code K____) fields may be blank or contain BD, BE, BS, CN, CV, FC, or RI. For covered hoppers, car type code C____, fields must be blank. NOTE: FC for car type code C____, when equipped.

4	30-34	Blank	All
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4	35	Articulated	All *
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An articulated car consists of two or more cars permanently coupled together in such a manner that they cannot be separated for operation in interchange service as individual cars. Such cars will be operated under one (1) reporting mark and one (1) number and, therefore, considered as one car in accordance with provisions of Circular No. OT-38-Series. Not applicable to car type codes P____ and Q____.

Report alpha/numeric code indicating number of units permanently coupled.

MINIMUM—2

A-10, B-11, C-12, D-13, E-14, F-15, G-16, H-17, I-18, J-19, K-20, L-21, M-22, N-23, O-24, P-25, Q-26, R-27, S-28, T-29, U-30, V-31, W-32, X-33, Y-34, Z-35.

RELATED FIELDS

1. Year and Month—Report for oldest car of set.
2. Original cost—report the total of the original costs as provided on Line 1, Cols. 43-48.
3. Indicator for reporting positive or negative additions or betterments as provided on Line 1, Col. 49.
4. Additions and Betterments—report the total costs as provided on Line 1, Cols. 50-54.
5. Ledger Values—report the total of the costs as provided on Line 1, Cols. 55-60. For articulated and multi-unit intermodal cars T.O.F.C. and C.O.F.C. Car Type Codes Q____. See items 11 to 18, otherwise use 8, 9 and 10 directly below.
6. Side and end door dimensions—report the width and height dimensions for the smallest car of the set.
7. Nominal Capacity, Tare Weight, Total Weight on Rail, Axles, Truck Type and Axle Spacing, Truck Center Length, Load Limit:
 - a) Nominal Capacity: (Gallons, Cubic Feet Capacity Pounds)—report the lowest nominal for a single unit in the set.
 - b) Tare Weight—report the average weight of the unit in the set.
 - c) Total Weight on Rail—report the lowest allowable weight of the unit in the set.
 - d) Axle—report the actual number of axles of the set.
 - e) Truck Type and Axle Spacing—report the longest length for a single unit in the set.
 - f) Truck Center Length—report the longest length of a single unit of the set.
 - g) Load Limit—report the lowest of the set.

UMLER SPECIFICATION MANUAL—SECTION I—CONTINUED

8. Outside Length—report the maximum coupled length of the set.
 9. Outside Dimensions—(Extreme Width, Height and Upper Eaves)—report the dimensions of the largest unit of the set.
 10. Inside Dimensions—report the dimension for the smallest unit of the set. Articulated and multi-unit intermodal cars T.O.F.C. and C.O.F.C. Car Type Code Q _____.
 11. a) Nominal Capacity-Card 3 (Cols. 24-26)—report in thousands of pounds the total capacity of the consist (set) as stenciled on the unit as is prescribed in Interchange Rule 70.
 b) Tare Weight-Card 3 (Cols. 27-30)—this is the combined light weight of the total number of units which make up the set.
 c) Total Weight on Rail-Card 3 (Cols. 41-43)—report the lowest allowable weight of the unit in the set.
 d) Axles-Card 3 (Col. 50)—report the actual number of axles of the consist.
 e) Truck Type and Axle Spacing Card 3 (Col. 51)—report the longest length for a single unit of the set.
 f) Truck Center Length-Card 3 (Cols. 45-48)—report the longest length of a single unit of the set.
 12. Outside Length—report the maximum coupled length of the set.
 13. Outside Dimensions—(Extreme Width, Height and Upper Eaves)—report the dimensions of the largest unit of the set.
 14. Inside Dimension—report the dimensions of the smallest unit of the set.
 15. Load Limit-Card 3 (Cols. 67-70)—AAR Manual of Standards and Recommended Practices-Notes to Figure 9-Note L:
 L. Load limit per unit to be stenciled on car, an exception to procedures used on single cars. Method for determining load limit per unit will be to determine lightweight of every unit and subtract this from the axle or wheel capacity, whichever is less, under the specific unit. The lowest figure so obtained should be used as the load limit for all units.
 16. A Unit-Card 4 (Cols. 39-52)
 a) Loading Capacity-Card 4 (Col. 39)-Trailer (Col. 39)—refer to Codes.
 b) Loading Capacity-Card 4 (Col. 39)-Trailer (Col. 40)—refer to Codes.
 c) Load Limit-Card 4 (Cols. 41-44)—maximum permissible weight that can be loaded into unit calculated by deducting lightweight of unit from wheel or axle capacity.
 d) Tare Weight-Card 4 (Cols. 45-47)—report tare weight of a unit.
 e) Total Weight on Rail-Card 4 (Cols. 48-50)—report the actual weight allowable on rail based on journal size (AAR Manual of Standards and Recommended Practices), wheel size or car construction wheel truck. The total weight on rail is proportional to the number of axles under the car.
 f) Axles-Card 4 (Col. 51)—report the number of axles in the A unit.
 g) Wheel size-Card 4 (Col. 51)—report wheel size which is standard to car.
 17. B Unit-Card 4 (Cols. 53-56)—fields and requirements will be same as A unit.
 18. C Unit-Card 4 (Cols. 67-70)—there could be multiple C units. If there is more than one unit and the C units are not identical, the lowest or most restrictive value should be used. Fields and requirements will be the same as A unit.

4	36	Body Type	All except Flats
		Container Material	Flats

Alphabetic code pertaining to the exterior of car, or permanent container on flat car with fitting code CN (Line 4, Cols. 24-29). Otherwise leave flat car blank.

A—Aluminum
 S—Standard Steel
 T—Stainless Steel
 W—Wood
 E—Combination

If the body or container is constructed of a mixture or combination of these materials, then the symbol E must appear.

4	37	Floor Type	Box, Flat, Gond., Ref., Std.
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Alphabetic or numeric code showing type of flooring. See Exhibit H for list of codes.

Box, Flat If numeric, MINIMUM—1, MAXIMUM—7
 If alpha, must be C, D, N, P, S, W or X.

EXCEPTION: Cars with AAR car type codes F____, G____, L____ and L____ may be blank.

Gondola If numeric, MINIMUM—1, MAXIMUM—7
 If alpha, must be A, C, D, N, P, S, W or X.

EXCEPTIONS: (1) If GB type car with steel floor (AAR car type codes G____, L____ or G____, L____) one of the following codes must be reported: N, 3, P, 4, S, 5. (2) If GB type car with wood floor (AAR car type codes G____, L____ or G____, L____) one of the following codes must be reported: C, 1, D, 2, W, S.

Refrigerator If numeric, MINIMUM—1, MAXIMUM—9
 If alpha, must be C, D, N, P, S, W or X.
 Stock Must be S or W.

4	38	Floor Trough	Flat, Gond.
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A one position alphabetic code indicating the car's floor characteristics. Can be blank.
 C=cradles
 T=trough type floor

4	39-42	Top Deck Height	Flat(Multi-Level)
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Numeric, measurement from the top of rail to top deck on which vehicles are loaded.
 Feet in Cols. 39-40, inches in Cols. 41-42 for all V____ except V4____, V9____, V5____ to V8____
 MINIMUM—0900 MAXIMUM—1103
 V0____ to V3____ MINIMUM—1206 MAXIMUM—1406

4	39	Lining	Box, Gond., Hop.
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Alphabetic code to show the type of lining used in the car. See Exhibit E for list of codes.
 Box Must be L, W, N, U, or S. If lining is mixed material, then the symbol for the majority must appear.

Gondola Blank or L, S, or W.
 Hopper Blank or L, M, P, R, S, V, or W.

4	39	Trailer Loading Capacity "A" Unit	Flat (FCA)
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•• Alphabetic code indicating the loading capacity of the "A" unit.
 0—Not suitable for trailer loading
 1—40' trailer
 2—40' to 45' trailer
 3—40' to 48' trailer
 4—40' to 50' trailer
 5—26' to 40' trailer
 6—26' to 45' trailer
 7—26' to 48' trailer
 8—26' to 50' trailer
 9—Not used

4	40	Container Loading Capacity "A" Unit	Flat (FCA)
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•• Alphabetic code indicating the loading capacity of the A unit.

- 0—Not suitable for container loading
 1—One (1) 40' container
 2—One (1) 40' container or two (2) 20' containers
 3—Two (2) 40' containers—stacked
 4—Two (2) 40' containers—stacked, or two (2) 20' containers and one (1) 40' container—stacked
 5—One (1) 35' container
 6—One (1) 45' container
 7—One (1) 40' container, and one (1) 45' container—stacked
 8—One (1) 40' container, and one (1) 48' container—stacked
 9—Two (2) 48' containers—stacked
 A—Two (2) 45' containers—stacked
 B—One (1) 45' container, and one (1) 48' container—stacked
 C—Two (2) 35' containers—stacked

4	40	Roof	Box, Gond., Hop.
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A one position numeric code showing roof characteristics of car. This field may be blank. Codes are as follows:

- Hopper (LO) (HKR) (HTR) 1=Trough hatch in roof
 Gondola, Equipped Hopper 2=Removable roof

- Gondola 3=Self-storing roof
 Box (LC) Hopper (LO) (HTR) 4=Standard roof hatch
 Box 5=Other types of openings

4	41	Outlets	Gond., Hop.
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Numeric code 1 thru 9 indicating the number of discharge outlets in the bottom of the car. (AAR car type code E____ or G____) must be zero if no outlets.

- 1=1 outlet 7=7 outlets
 2=2 outlets 8=8 outlets
 3=3 outlets 9=9 or more outlets
 6=6 outlets

4	41-44	Load Limit "A" Unit (00)	Flat (FCA)
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•• Numeric, report in hundreds of pounds the "stenciled load limit of the unit."
 MINIMUM—055 MAXIMUM—100

*Note: The method for determining load limit per unit will be to determine the lightweight of each unit and subtract this from the axle or wheel capacity, whichever is less, under the specific unit.

4	42	Compartments	Flat
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Numeric code 1 thru 9 indicating the number of compartments or bays in the car.

- 1=1 compartment 4=4 compartments 7=7 compartments
 2=2 compartments 5=5 compartments 8=8 compartments
 3=3 compartments 6=6 compartments 9=9 or more compartments

4	43	Floor Strength	Box, Gond.
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Numeric code indicating the floor strength characteristics of car in accordance with Mechanical Interchange Rule 88. Report zero or leave blank if car floor does not meet general service standards.

- 0=Floor of less than 25,000 lbs. capacity
 2=Floor of 25,000 lbs. capacity
 5=Floor of 50,000 lbs. capacity

4	43-48	(RR) Superstructure Cost (\$)	Flat (FCA)
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Report in U.S. dollars the original cost of the superstructure. (Raise cents to the nearest dollar.) Applicable to all car type codes V____ except, privately-owned or railroad-owned cars capitalized as an integral part of the car. See Line 1, Cols. 55-50. This field can be blank for railroad-owned superstructures.

Note: May be blank, if Line 4, Col. 55 is equal to N.
 MINIMUM—000100

4	44-45	Side Door Type	Box, Gond., Hop.
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Numeric, code to indicate type of side door.

- Box, Refrigerator, Stock Use one of the list below:
 Gondola (G____ only) Use code 13 or 15

CODE	DESCRIPTION	CODE	DESCRIPTION
01	Single Regular doors, staggered	08	Combination Regular and Plug Doors, staggered
02	Single Plug Doors, centered	09	Combination Regular and Plug Doors, staggered
03	Single Plug Doors, staggered	10	Split Refrigerator Door (hinged)
04	Double Regular Doors, centered	11	More than one opening on same side
05	Double Regular Doors, staggered	12	Overhead Doors
06	Double Plug Doors, centered	13	Other
07	Double Plug Doors, staggered	14	Single Regular Doors, centered
		15	Permanently closed or no side door

4	45-47	Tare Weight "A" Unit (00)	Flat (FCA)
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•• Numeric, the actual lightweight reported in hundreds of pounds. Round up to nearest 100. Defined in AAR Interchange Rule 70, e.g. actual 23550 report as 0235. Do Not Report Average Tare Weight For Consist.
 MINIMUM—230 MAXIMUM—100

4	48-50	Blank	Flat (FCA)
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4	48-50	Total Allowable Weight on Rail "A" Unit (000)	Flat (FCA)
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•• Numeric, report the lowest weight allowable on rail based on the smallest component of Manual of Standards & Recommended Practices, wheel size, car construction, etc.
 MINIMUM—142 MAXIMUM—100

UMLER SPECIFICATION MANUAL—SECTION II—CONTINUED

1 27-30 Railroad Lessee's Mark Tank

Report the uniform alphabetic code of railroad. This field may be left blank, however, if reported, the reporting mark cannot be equal to the owner's field or be a family reporting mark.

1 31-32 Month Built (Underframe) Tank

Numeric, report the month the car was built or rebuilt, e.g., 01 = January. This field is mandatory.

EXCEPTIONS: If a railroad-owned car, and placed into service secondhand prior to January 1, 1973, report the month placed into service.

MINIMUM—01 MAXIMUM—12

1 33-34 Year Built Underfr. (New) Tank

Last two (2) digits of year built, e.g., year built 1981 report 81 in this field. This field must never be blank.

1 35-36 Year Rebuilt Tank

Last two (2) digits of the year rebuilt. If not applicable report zeroes. Cannot be equal to the year built new.

1. Railroad Cars-applicable only to cars meeting status as provided in both ICC Accounting Rules, and the AAR Mechanical Interchange Rule 88, Office Manual.
2. Private cars-applicable only to cars meeting status as provided in Mileage Tariff 6007-Series, Rule 4, Item 195 and AAR Mechanical Interchange Rule 88, Sections B and D, Office Manual and Sections A and B, Field Manual. For the purpose of calculating the cars' mechanical age and mileage rate, the reported year rebuilt governs.

* NOTE: Do not report rebuilt year unless car has been approved by the Secretary, Mechanical Division, AAR.

1 37-38 Private Tank Year RR Yr 2nd hand/ICC Rebuilt Year Tank

1. For privately owned tank cars on a straight mileage basis of settlement report the last two (2) digits of year tank was built. If tank is 31 years or over, the car, for the purpose of calculating a mileage rate will be considered over 30 years regardless of the fact that the underframe may be newer.

2. For railroad owned tank cars on a car hire basis of settlement *leave blank* if cars are new. For railroad tank cars on a car hire basis of settlement, use this field to report year acquired second hand (denotes change in ownership). AAR Certification Form must accompany submission.

3. Railroad cars built prior to January 1, 1973—applicable only to cars meeting status as provided in ICC Accounting Rules but not meeting rebuilt standards prescribed in AAR Interchange Rules.

NOTE 1: Car Hire charges will be calculated from the year new of underframe or year second-hand prior to January 1, 1973.

NOTE 2: All secondhand acquisitions must be accompanied by one of the applicable AAR certification forms prescribed in AAR Mechanical Interchange Rule 88, Office Manual.

1 39-42 Advance Installation Date Tank

Month and year car is to be installed into service. Report month in Cols. 39-40, year in Cols. 41-42, e.g., October 1983 report 1083. Do not report the current month and year unless it is the installation date. If left blank or date less than current is provided, the current date will be inserted by the AAR. This field is relative to the reported estimated tare weight information, see Line 3, Col. 31-34.

Cars may be advance registered up to one (1) year, e.g., in January 1983 the maximum advance registration date will be 0184. Question: Do expired installation dates have to be re-advanced—Yes! Question: If a car is placed into interchange before the advanced installation date do I have to submit an update—Yes! Question: Is the effective date of the rate or the advanced installation date in the output tape. Answer: The advance installation date.

1 43-48 Original Cost (\$) (RR) Tank Original Value (\$) (Pvt) Tank

THIS IS A MANDATORY FIELD FOR ALL RAILROAD-OWNED CARS. IT IS MANDATORY FOR ALL PRIVATELY-OWNED CARS BUILT IN 1968 AND SUBSEQUENT YEARS AND IS USED TO CALCULATE THE APPLICABLE MILEAGE RATES FOR SUCH PRIVATE TANK CARS, IF AN ADJUSTED VALUE WAS NOT REPORTED.

1. For railroad-owned cars, report in U.S. dollars the original ledger value of original owner or acquisition cost for cars purchased prior to January 1, 1973. For cars rebuilt prior to January 1, 1973, report the cost as prescribed in the ICC Accounting Rules. For cars rebuilt after January 1, 1973, report the cost prescribed in AAR Interchange Rule 88. (Exclude additions and betterments).

EXCEPTIONS:

- For cars acquired secondhand prior to January 1, 1973, the Original Owner's cost or the acquisition cost may be greater than the current owner's ledger value.
- For cars to which retirement of car's components, e.g., running boards; coils removed; the ledger value is reduced and therefore may be less than Original Cost which is never altered.
- For privately-owned cars built in 1967 and prior, report zeroes as the AAR will calculate the value from the Base Classification Value Description in Line 2, Cols. 49-54 and insert here in its master file.
- For privately-owned cars built in 1968 and subsequent years, report in U.S. dollars the original value or fair market-investment tax credit value as provided in Mileage Tariff PHJ 6007-Series.

EXCEPTION: For cars to which retirement of car's components, e.g., running boards; coils removed; the ledger value or adjusted value (Cols. 55-60) is reduced and therefore may be less than Original Value, which is never altered.

NOTE: Raise all cents to the next dollar, e.g., \$5,501.02 = 005502.

MINIMUM—000100 MAXIMUM—

1 49 Indicator for Positive/Negative A&B Tank

This field must be reported when reporting additions or betterments Line 7, Col. 55-54, otherwise report blank.

P—Indicates the addition of a capitalized cost of additions or betterments made subsequent to its installation into service.

N—Indicates the subtraction of a capitalized cost of additions or betterments that were moved from the car subsequent to its installation into service.

1 50-54 Additions & Betterments Tariff Additions (\$) RR Tank (Pvt) Tank

- For railroad-owned cars report in U.S. dollars the sum of the costs for:
 - Additions are new parts applied subsequent to the date the car was built or rebuilt carried in the capital investment account.
 - Betterments are improvements of parts of existing equipment through the installation of superior parts for inferior parts replaced.
- Do not report additions and/or betterment costs applied at the time of DOT DOT rehabilitations. These costs will be added to the ledger value following the completion of the car's 30th year.
- For privately-owned cars built in 1967 and prior, report in U.S. dollars capitalized additions and betterments not already included in the BCV description.
- For privately-owned cars built in 1968 and subsequent, report in U.S. dollars the total of any into-service freight charges, capitalized original lining, capitalized additions and betterments, if applicable, and not already included in the original owner's original fair market value, as provided in Tariff 6007-Series.

NOTE: Raise all cents to the next dollar, e.g., \$5,501.02 = 005502.

MINIMUM—00000

MAXIMUM—

1 55-60 Ledger Value Adjusted Value RR Tank (Pvt) Tank

THIS FIELD IS USED TO CALCULATE CAR HIRE AND MILEAGE CHARGES.

1. For railroad-owned cars report in U.S. dollars the original cost (Cols. 43-48) plus additions and betterments (Cols. 49-54).

EXCEPTION: For cars covered under OT-37 Series report original cost plus additional betterment costs prior and subsequent to rehabilitation. Do not include addition and betterment costs that occurred at the time of rehabilitation.

2. For cars acquired secondhand prior to January 1, 1973 report the original cost of acquisition cost and subsequent addition and betterment costs, and for cars purchased subsequent to December 31, 1972, report the original cost of the original owner and subsequent betterment costs.

3. For privately-owned tank cars built in 1967 and prior; report in U.S. dollars the original cost plus tariff additions (Cols. 49-54), if applicable, plus the Base Classification Value (Cols. 49-54) calculated by the AAR. If tariff additions are applicable and the BCV value is reported, the AAR will calculate the adjusted value. If tariff additions are not applicable report zero. The AAR will insert the BCV Value.

4. For privately-owned tank cars built in 1968 and subsequent; report in U.S. dollars the original value (Cols. 43-48) plus tariff additions (Cols. 49-54). If tariff additions are applicable report the same as original value (Cols. 43-48) to this field.

NOTE: Raise all cents to the next dollar, e.g., \$5,501.02 = 005502.

1 61 Indicator Tank

A one position Alpha/Numeric field:

- Car in error with zero rate. (Inserted by AAR)
- Car is registered with an estimated value and subject to mileage allowances per Tariff PHJ 6007-Series.
- Car subject to mileage allowances, per Tariff PHJ-6007-Series.
- Car is subject to car hire charges rates.
- Car subject to car owner's designated rate.
- Car subject to reduced car hire rates in accordance with Ex Parte No. 224, Sub. 1 (see Exhibit 5)
- Car is subject to special lease arrangement with Car Hire or Mileage Allowances other than Normal Basis or has transportation code S, X or Y. Zero rate inserted in this field.
- Railroad car subject to car hire charges being reported with an estimated value for AAR auditing purposes.
- Car subject to car hire (time/mileage) charges under the provisions of Circular OT-37 Series having estimated costs.
- Car subject to car hire (time/mileage) under the provisions of Circular OT-37 Series.
- Cars subject to car hire (time/mileage) under the provisions of Circular OT-37 Series having estimated addition/betterment costs reported subsequent to car's installation. Not applicable to rebuilt cars.

NOTE: ONCE THE INDICATOR HAS BEEN CHANGED TO ZERO, THE CAR OWNER MUST REINSTATE THE APPROPRIATE INDICATOR CODE.

1 62-80 Blank Tank

Line 2 Col. 1 must be 2, Cols. 2-18 must equal Line 1.

2 19 Clearance Tank

Alphabetic code to denote plate clearance of car (see Exhibit F).

Report B—If clearance does not exceed Plate B.

Report C—If clearance is greater than Plate B, but does not exceed Plate C.

Report E—If clearance is greater than Plates B and C, but does not exceed Plate E.

Report F—If clearance is greater than Plates B, C and E, but does not exceed Plate F.

Report G—If clearance exceeds Plates B, C, E and F.

NOTE: C-E-F must agree with similar stenciling on side of car.

G must agree with stenciling on side of car that exceeds plate F.

2 20-24 Outside Dimensions—Length Tank

Numeric, distance over pulling faces of couplers in normal positions. Feet in Cols. 20-22, inches in Cols. 23-24. Round fraction to the higher inch, e.g., 05' 1/4" = 06.

MINIMUM—026 06

MAXIMUM—

UMLER SPECIFICATION MANUAL—SECTION II—CONTINUED

2	25-28	Outside Dimensions—Extreme Width	Tank
<p>Numeric, measurement of the extreme outside width of car including attachments projecting to greatest extent. Must agree relationally with height of extreme width above rail for clearance code reported (See Exhibit F to cross check these two fields). Feet in Cols. 25-26, inches in Cols. 27-28. Round fraction to the higher inch, e.g., 05 1/4" = 06.</p>			
clearance is B	MINIMUM—07 00	MAXIMUM—10 10	
clearance is C	MINIMUM—07 00	MAXIMUM—10 08	
clearance is E	MINIMUM—07 00	MAXIMUM—10 08	
clearance is F	MINIMUM—07 00	MAXIMUM—10 08	
clearance exceeds Plates B,C,E,F (Code G)	MINIMUM—07 00	MAXIMUM—11 02	

2	29-32	Outside Dimensions—Hgt. of Ext. Width	Tank
<p>Numeric, measurement is from top of rail to the highest point where the extreme width occurs. Feet in Cols. 29-30, inches in Cols. 31-32. Round fraction to the higher inch, e.g., 1 1/4" = 06.</p>			
clearance is B	MINIMUM—01 00	MAXIMUM—15 01	
clearance is C	MINIMUM—01 00	MAXIMUM—15 06	
clearance is E	MINIMUM—01 00	MAXIMUM—15 09	
clearance is F	MINIMUM—01 00	MAXIMUM—17 00	
clearance exceeds Plates B,C,E,F (Code G)	MINIMUM—01 00	MAXIMUM—17 11	

2	33-36	Outside Dimension—Extreme Height	Tank
<p>Numeric, height from top of rail to extreme projecting height. Feet in Cols. 33-34, inches in Cols. 35-36. Round fraction to the higher inch, e.g., 05 1/4" = 06.</p>			
clearance is B	MINIMUM—08 00	MAXIMUM—15 01	
clearance is C	MINIMUM—08 00	MAXIMUM—15 06	
clearance is E	MINIMUM—08 00	MAXIMUM—15 09	
clearance is F	MINIMUM—08 00	MAXIMUM—17 00	
clearance exceeds Plates B,C,E,F (Code G)	MINIMUM—08 00	MAXIMUM—17 11	

2	37-48	Shipping Container Specifications	Tank
<p>Report the approved DOT or AAR Container Specifications approved for the type service the car is operating. (See Exhibit M). Do Not Report Changes on Select Field Format. Do not report prefixes, e.g., ICC103AL = 103AL, DOT103ALW = 103ALW.</p>			

2	49-62	Base Classification Value Descrip. (BCV)	Tank
<p>Report description that establishes the AAR Base Classification Value which is based upon the depreciated reproduction value, as detailed below:</p>			

2	49-50	BCV—Shell Capacity (000)	Tank
<p>Report shell full capacity in increments in thousands of gallons. Amounts of 500 gallons or more will be reported as an additional thousand and amounts less than 500 gallons will be reported to the lower thousands. LEAVE BLANK FOR MAJOR CLASSES 17, 58 and 77.</p>			
		MINIMUM—01	MAXIMUM—61

2	51	BCV—Compartments	Tank
<p>Report number of compartments in tank. LEAVE BLANK FOR MAJOR CLASSES 03, 17, 58 and 77.</p>			
		MINIMUM—1	MAXIMUM—9 or more compartments

2	52-54	BCV—Nominal Truck Capacity	Tank
<p>Report in even tons the nominal truck capacity. FOR MAJOR CLASSES 03 and 77 report 000. Must be one of the following: 000, 030, 040, 050, 070, 100, 125, 140, 150, 200.</p>			
<p>NOTE: If car equipped with double 70-ton trucks, report as 140. If car equipped with double 10-ton trucks, report as 200.</p>			

2	55	BCV—Bearings	Tank
<p>Report plain bearings and roller bearings.</p>			

2	56	BCV—Insulation	Tank
<p>Report insulation thickness. BLANK FOR UNEQUIPPED.</p>			
		+4 inches or less	
		+5-7 inches	
		+8 inches or more	

2	57	BCV—Type Insulation	Tank
<p>Report type of insulation if equipped (Col. 56). BLANK FOR UNEQUIPPED.</p>			
		+Cork (applies to single compartment car only).	
		+Foam (thickness, Col. 56, to be converted where applicable to the equivalent in inches based on 2 1/2 pounds density per cubic foot of foam).	
		+General (glass fiber and all other types).	

2	58	BCV—Lines of Coil	Tank
<p>Report lines of coil. BLANK FOR UNEQUIPPED.</p>			
		+1-7 lines of coils.	
		+8-15 lines of coils.	
		+16 or more lines of coils.	
<p>Lines of interior coils should be expressed in two-inch pipe size equivalents by using these conversion factors:</p>			

3-inch pipe: number of lines of coil times 1.47
 4-inch pipe: number of lines of coil times 1.89
 5-inch pipe: number of lines of coil times 2.34
 6-inch pipe: number of lines of coil times 2.79
 8-inch pipe: number of lines of coil times 3.62

EXAMPLE: Car equipped with 7 lines of 3-inch coils, multiply 7x1.47 = 10.29, and a "2" would be used to indicate 8 to 15 lines.

2	59	BCV—Coils Exterior/Interior	Tank
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E = exterior lines of coils (Col. 58).

I = interior lines of coils (Col. 58).

BLANK FOR CARS NOT EQUIPPED WITH COILS.

2	60	BCV—Coil Material	Tank
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Report coil material if equipped (Cols. 58 & 59). BLANK FOR CAR NOT EQUIPPED WITH COILS.

A = Aluminum

M = Monel

B = Brass

N = Nickel

C = Carbon steel

S = Stainless steel

I = Inconel

2	61	BCV—Lining Material	Tank
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Report lining material. BLANK FOR UNEQUIPPED.

B = Nickel electro-plating, e.g., Bart. (Not applicable to Major Class 19).

G = Glass (any thickness) only applies to Major Class 03.

K = Electroless plating, e.g., Kanigen.

L = Lead lined (any thickness).

R = Rubber (applied in sheet form). Applicable to Carbon Steel cars only.

S = Stainless lining only applies to Major Class 03.

T = Paint, Phenolic, Epoxy and metal spray deposits such as Lithcoat, Amercoat, Harsite, PPG Columbia #7 Neoprene Rubber Latex.

V = Poly-Vinyl Chloride.

2	62	BCV—Auxiliary Features	Tank
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Report auxiliary features. BLANK FOR UNEQUIPPED.

A = Alloy fittings (applicable to Major Classes 05, 10 and 11, and then only if car is lined (Col. 61). Alloy fittings being described as a combination of any 3 or more of the following:

1. Alloy manway cover.
2. Alloy manway ring.
3. Solid alloy safety valve or vent.
4. Solid alloy top unloading valve.
5. Solid alloy bottom unloading valve.
6. Manway ring and bottom outlet lined with 316, 316L stainless steel or nickel.

D = Applies to Major Class 54-57 (DOT 120A300-600W, 120J600W) and must be equipped with both pressure and non-pressure fittings.

E = Destination Heater—electric system for heating tank (not applicable where only applied to outlet valve or siphon pipe).

H = In-Transit heater.

Z = Low temperature steel. Only applies to Major Classes 38 through 49, 53 through 57 and 59 through 65.

NOTE: Special design features and fittings required to transport the following commodities.

C = Carbon dioxide. If applicable, only for Major Classes 56 or 57.

F = Edible food paste.

L = Anhydrous Hydrogen Chloride. If applicable, only for Major Classes 56 or 57.

X = Liquid Oxygen, (applicable for Major Class 03 only).

2	63-80	Blank	Tank
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Line 3, Col. 1 must be 3. Cols. 2-18 must equal 1 and 2.

3	19-23	Actual Shell Capacity	Tank
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Report the actual gallonage capacity of the shell. Report U.S. gallons only. The gallonage reported will be compared to the gallonage reported in the BCV Description, Line 2, Cols. 49-50, therefore, must be within 500 gallons of the BCV gallons, otherwise it will be considered in error. Homogeneous car series having equal gallons capacity, except major classes 67, 76 and 77 advance registered cars or newly reported cars will be considered in error.

3	24-26	Nominal Capacity (000)	Tank
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Numeric capacity in thousands of pounds as stenciled on car and defined in AAR Interchange Rule 70.

MINIMUM—060

MAXIMUM—400

3	27-30	Tare Weight (00)	Tank
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DO NOT REPORT AN AVERAGE TARE WEIGHT FOR CAR SERIES EXCEPT ADVANCE REGISTRATIONS.

Numeric, the actual light weight (tare) in hundreds of pounds for each car.

Reporting instructions, as defined by AAR Interchange Rule 70, e.g., actual = 17550, report 0175; actual = 17551, report 0176.

MINIMUM—0310

MAXIMUM—2000

NOTE: When reporting new cars (except advance registrations) and cars that have been reweighed the weighing road (Cols. 31-34) and weighing date (Cols. 35-40) must be reported.

UMLER SPECIFICATION MANUAL—SECTION II—CONTINUED

3	31-34	Weighing Road	Tank
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1. Car weighed or reweighed by a railroad report uniform alphabetic code of weighing road.
2. Car weighed or reweighed by owner report owners reporting mark.
3. Car weighed or reweighed by other than owner or railroad report owners mark.
4. When reporting new cars or advanced registered cars and the tare weight is an estimated weight report EST. EST will be indicated as an error 60 days after the service date, (Line 1, Cols. 39-42) became current.
5. For new cars, the word "NEW" can be substituted for company mark.

NOTE: When reporting new cars (except advance registrations) and cars that have been reweighed, the tare weight (Cols. 27-30) and the weighing date field (Cols. 35-40) must be reported.

3	35-40	Weighing Date	Tank
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Report as one (1) complete six (6) position, numeric, data field.
Month (35-36), day (37-38) and year (39-40) the car was weighed or reweighed in accordance with AAR Interchange Rule 70.

NOTE 1: When reporting new cars and cars that have been reweighed the tare weight (Cols. 27-30) and the weighing road (Cols. 31-34) must be reported.

NOTE 2: When reporting new or advanced registered cars and the tare weight is an estimated weight report zeros.

3	41-43	Total Allowable Weight on Rail (000)	Tank
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Numeric, the actual total weight allowable on rail based on journal size (AAR Manual of Standards & Recommended Practices), wheel size or car construction, wheel truck, 4 axes per car.

Journal Size	4 1/4 x 8	5 x 9	5 1/2 x 10	6 x 11	6 1/2 x 12	7 x 12
Total Wgt. on Rail (000)	103	142	177	220	263	315
AAR Axle Code	B	C	D	E	F	G
Nominal Capacity (000)	60	88	110	154	200	250
Axle Spacing	5'6"	5'8"	5'6"	5'8"	5'10"	6'0"
Axle Rail Load—lbs.	25750	35500	44250	55000	65750	78750

- NOTE:
1. Total weight of 1 million pounds or more—999
 2. For cars having 6-wheel, 8-wheel, etc. trucks, the total weight on rail is proportional to the number of axes under car.
 3. Starred cars, because of construction, wheels, springing etc. must have the total weight on rail applicable for the exception. The reported weight must be less than the standard weights indicated above.

3	44	Star Symbol	All
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** When the car owners in accordance with the provisions of Interchange Rule 70 have reduced the nominal capacity and/or load limit of a car. The code "S" must be reported.

When an owner has in accordance with the provisions of Rule 70 restricted, because of defects or structural weaknesses, the maximum loading capacity allowed by the journal size of a car. The code "R" must be reported. The load limit of such cars cannot be increased without being reviewed by the AAR Mechanical Division. If not applicable leave blank.

3	45-48	Truck Center Length	Tank
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Numeric, distance between pins in the truck center plates. This dimension must be reported for cars with an outside length (Line 2, Cols. 20-24) of 62' 06" or greater. Reporting is optional for cars with an outside length of 62' 05" or less. Feet in Cols. 45-46, inches in Cols. 47-48. Round fraction to lower inch, e.g., 05 3/4" = 05.
MINIMUM—18 00 MAXIMUM—76 11

3	49	Bearing & Brake Shoe Type	Tank
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Alphabetic code indicating the type of journal bearings and brake shoes.

- A-Plain bearings and composition brake shoes
- B-Roller bearings and composition brake shoes
- C-Plain bearings and cast iron brake shoes
- D-Roller bearings and cast iron brake shoes
- E-Roller bearings, composition brake shoes and constant contact side bearings
- F-Roller bearings, cast iron brake shoes and constant side bearings
- G-Roller bearings, composition brake shoes, and empty/load brake system
- H-Roller bearings, composition brake shoes, constant contact side bearings, and empty/load brake system
- I-Roller bearings, cast iron shoes, and empty/load brake system
- J-Roller bearings, cast iron shoes, constant contact side bearings, and empty/load brake system

Note: Cars having plain bearings codes A or C will be edited to insure compliance with AAR Interchange Rules. Not applicable to car type codes D, M, N, Z and U. The edit criteria is as follows: Cars having plain bearing codes, Line 3, Position 49, "A" or "C" will be flagged in error should the code not comply with proper timeframes.

Codes	Codes	Date Bkt/Rbt	Condition
Plain Bearing (3, 49)* Axle (3, 41-43)			
1. A, C	B, C, D, E (New) 8/1/66 & Subsequent	Error	
2. A, C	F, G (Rbt) 8/1/66 & Subsequent	Error	
3. A, C	B, C, D, E (Rbt) 1/1/70 & Subsequent	Error	
4. A, C	F, G (New) 8/1/66 & Subsequent	Error	

The code A or C may be present in the record if the transportation code is equal to XJ. Then normal editing will prevail.

3	50	Axes	Tank
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Report the applicable numeric code indicating the number of axes per car.

3	51	Truck Type & Axle Spacing	Tank
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1. Alphabetic code indicating truck type and spacing with devices, to control stability of freight cars in accordance with Rule 88B, Item 9, a, Field Manual and Rule 88, Section B. Spring (Truck), Office Manual, AAR Interchange Rules, and distance of axle spacing per standard two (2) axle truck.

A = 5 feet, 1 inch	D = 5 feet, 8 inches
B = 5 feet, 3 inches	E = 5 feet, 10 inches
C = 5 feet, 6 inches	F = 6 feet
	G = Non-standard dimensions

2. Alphabetic code indicating truck type and spacing without snubber devices and axle spacing per standard two (2) axle truck.

J = 5 feet, 1 inch	M = 5 feet, 8 inches
K = 5 feet, 3 inches	N = 5 feet, 10 inches
L = 5 feet, 6 inches	P = 6 feet
	Q = Non-standard dimensions

3. Cars not having standard trucks report blank.

3	52	Wheel Size	Tank
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Numeric code indicating the wheel size which is standard to the car according to designation in Rule 41, Section E, AAR Interchange Rules.

1 = 28 inch Wheel	4 = 36 inch Wheel
2 = 30 inch Wheel	5 = 38 inch Wheel
3 = 33 inch Wheel	6 = 33 and 36 inch Wheels (Interchangeable Only)

NOTE: To report code 6, the car must be registered as an articulated car. Line 4, Col. 19 must be reported.

3	53-54	Draft Gear/Couplers	Tank
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Report numeric code INDICATING draft gear or coupler

- 66—Standard Draft Gear with F Bottom Shelf (only) couplers.
- 77—Standard Draft Gear with E, F or E/F Double Top and Bottom Shelf Couplers. (Shelves for cars 105, 112, 114, J, S or T and/or other tank cars carrying placarded materials.)
- 88—Standard Draft Gear with Standard E couplers without shelves.

All others report number of inches of travel in draft gear or center sill of car with underframe from normal position to maximum extension for one end of car.
Inches of travel: MINIMUM—05 MAXIMUM—11

3	55-80	Blank	Tank
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3	55-58	Blank	Tank
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3	59-62	Flexible Car Hire Time Charge	Tank
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Report the flexible time charge, must be less than the Ex Parte No. 334 rate and less than the sum of the Ex Parte No. 334 rate and the OT-37 supplemental charge.

3	63-66	Flexible Mileage Charge	Tank
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Report the flexible mileage charge, must be less than or equal to the Ex Parte No. 334 rate charge.

3	67-80	Blank	Tank
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3	67-80	Blank	Tank
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3	67-80	Blank	Tank
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3	67-80	Blank	Tank
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3	67-80	Blank	Tank
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3	67-80	Blank	Tank
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3	67-80	Blank	Tank
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3	67-80	Blank	Tank
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3	67-80	Blank	Tank
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3	67-80	Blank	Tank
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UMLER SPECIFICATION MANUAL—SECTION II—CONTINUED, SECTION III

5. Ledger Values—report the total of the costs as provided on Line 1, Cols. 55-60.
 6. Nominal Capacity, Tare Weight, Total Weight on Rail, Axles, Truck Type and Axle Spacing, Truck Center Length:
 a) Nominal Capacity (Gallons, Cubic Feet Capacity Pounds)—report the lowest nominal for a single unit in the set.
 b) Tare Weight—report the average weight of the set.
 c) Total Weight on Rail—report the lowest allowable weight of the unit in the set.
 d) Axles—report the average number of axles per unit on the car. Must be reported in whole numbers. Reduce fractions to the lower number.
 e) Truck Type and Axle Spacing—report the longest length for a single unit in the set.
 f) Truck Center Length—report the longest length of a single unit of the set.
 7. Outside Length—report the maximum coupled length of the set.
 8. Outside Dimensions (Extreme Width, Height and Upper Eaves)—report the dimensions of the largest unit of the set.

4	36-80	Blank	Tank
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SECTION III

SPECIFICATIONS AND APPLICABLE FORMATS AND LINE NUMBERS

TYPE: TOFC/COFC
 FORMAT: D
 LINES: 1,2,3,4

LINE	COLUMN NO.	FIELD DESCRIPTION	TOFC/COFC
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NOTE: Columns 2 through 18 are repeated on Lines 2 through 4.

1-4	1	Line Number	TOFC/COFC
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Must be 1,2,3, or 4.

1-4	2	Transaction Code	TOFC/COFC
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To DELETE a unit or unit series from the file, the transaction code must be a one (1). There are three methods that can be used: 1) submit the entire first line of information; 2) the complete tape record; or 3) submit UMLER deletion format (Exhibit C), e.g., (a.) for a single unit submit code 1, unit initials, low number (high number may be left blank), and applicable unit type code or four zeroes, (b.) for units in consecutively numbered series do not report the applicable unit type code; report the difference between the low and high number.
 To ADD a unit or consecutively numbered unit series to the file, the transaction code must be two (2).

A change in unit initials and/or numbers requires a complete ADD registration (transaction code 2) under the new initials and numbers and a DELETE (transaction code A) of the former initials and numbers. See Section I.

To CHANGE data (other than unit initials and numbers) on the file, the transaction code must be three (3). One of two methods can be used:
 A. Standard Method: Submit code three (3) in the complete line of information being submitted or the complete tape record.

B. Select Field Method: Transaction code must be a four (4). 1. This method can be used to change as many as five data fields for a single unit or consecutively numbered unit series. 2. On-line (TCAM) submitters can report up to ten (10) fields of information. (See Train Manual, UMLER section V, page 27). The Select Field Format (Exhibit B) must be used.

- Report unit initials.
- Report low number and the high number for consecutive unit series.
- Report line number in which data change is to be made.
- Report field identity number, i.e., the first column in which data are reported.
- Report data to be changed. See example below:

LINE	COLUMN NO.	FIELD DESCRIPTION	TOFC/COFC
1	1	Line Number	TOFC/COFC
2	2	Transaction Code	TOFC/COFC
3	3	Unit Initials	TOFC/COFC
4	4	Low Number	TOFC/COFC
5	5	High Number	TOFC/COFC
6	6	Unit Type Code	TOFC/COFC
7	7	Outside Length	TOFC/COFC
8	8	Truck Type and Axle Spacing	TOFC/COFC
9	9	Truck Center Length	TOFC/COFC
10	10	Outside Dimensions	TOFC/COFC
11	11	Outside Dimensions	TOFC/COFC
12	12	Outside Dimensions	TOFC/COFC
13	13	Outside Dimensions	TOFC/COFC
14	14	Outside Dimensions	TOFC/COFC
15	15	Outside Dimensions	TOFC/COFC
16	16	Outside Dimensions	TOFC/COFC
17	17	Outside Dimensions	TOFC/COFC
18	18	Outside Dimensions	TOFC/COFC
19	19	Outside Dimensions	TOFC/COFC
20	20	Outside Dimensions	TOFC/COFC
21	21	Outside Dimensions	TOFC/COFC
22	22	Outside Dimensions	TOFC/COFC
23	23	Outside Dimensions	TOFC/COFC
24	24	Outside Dimensions	TOFC/COFC
25	25	Outside Dimensions	TOFC/COFC
26	26	Outside Dimensions	TOFC/COFC
27	27	Outside Dimensions	TOFC/COFC
28	28	Outside Dimensions	TOFC/COFC
29	29	Outside Dimensions	TOFC/COFC
30	30	Outside Dimensions	TOFC/COFC
31	31	Outside Dimensions	TOFC/COFC
32	32	Outside Dimensions	TOFC/COFC
33	33	Outside Dimensions	TOFC/COFC
34	34	Outside Dimensions	TOFC/COFC
35	35	Outside Dimensions	TOFC/COFC
36	36	Outside Dimensions	TOFC/COFC
37	37	Outside Dimensions	TOFC/COFC
38	38	Outside Dimensions	TOFC/COFC
39	39	Outside Dimensions	TOFC/COFC
40	40	Outside Dimensions	TOFC/COFC
41	41	Outside Dimensions	TOFC/COFC
42	42	Outside Dimensions	TOFC/COFC
43	43	Outside Dimensions	TOFC/COFC
44	44	Outside Dimensions	TOFC/COFC
45	45	Outside Dimensions	TOFC/COFC
46	46	Outside Dimensions	TOFC/COFC
47	47	Outside Dimensions	TOFC/COFC
48	48	Outside Dimensions	TOFC/COFC
49	49	Outside Dimensions	TOFC/COFC
50	50	Outside Dimensions	TOFC/COFC
51	51	Outside Dimensions	TOFC/COFC
52	52	Outside Dimensions	TOFC/COFC
53	53	Outside Dimensions	TOFC/COFC
54	54	Outside Dimensions	TOFC/COFC
55	55	Outside Dimensions	TOFC/COFC
56	56	Outside Dimensions	TOFC/COFC
57	57	Outside Dimensions	TOFC/COFC
58	58	Outside Dimensions	TOFC/COFC
59	59	Outside Dimensions	TOFC/COFC
60	60	Outside Dimensions	TOFC/COFC
61	61	Outside Dimensions	TOFC/COFC
62	62	Outside Dimensions	TOFC/COFC
63	63	Outside Dimensions	TOFC/COFC
64	64	Outside Dimensions	TOFC/COFC
65	65	Outside Dimensions	TOFC/COFC
66	66	Outside Dimensions	TOFC/COFC
67	67	Outside Dimensions	TOFC/COFC
68	68	Outside Dimensions	TOFC/COFC
69	69	Outside Dimensions	TOFC/COFC
70	70	Outside Dimensions	TOFC/COFC
71	71	Outside Dimensions	TOFC/COFC
72	72	Outside Dimensions	TOFC/COFC
73	73	Outside Dimensions	TOFC/COFC
74	74	Outside Dimensions	TOFC/COFC
75	75	Outside Dimensions	TOFC/COFC
76	76	Outside Dimensions	TOFC/COFC
77	77	Outside Dimensions	TOFC/COFC
78	78	Outside Dimensions	TOFC/COFC
79	79	Outside Dimensions	TOFC/COFC
80	80	Outside Dimensions	TOFC/COFC
81	81	Outside Dimensions	TOFC/COFC
82	82	Outside Dimensions	TOFC/COFC
83	83	Outside Dimensions	TOFC/COFC
84	84	Outside Dimensions	TOFC/COFC
85	85	Outside Dimensions	TOFC/COFC
86	86	Outside Dimensions	TOFC/COFC
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91	91	Outside Dimensions	TOFC/COFC
92	92	Outside Dimensions	TOFC/COFC
93	93	Outside Dimensions	TOFC/COFC
94	94	Outside Dimensions	TOFC/COFC
95	95	Outside Dimensions	TOFC/COFC
96	96	Outside Dimensions	TOFC/COFC
97	97	Outside Dimensions	TOFC/COFC
98	98	Outside Dimensions	TOFC/COFC
99	99	Outside Dimensions	TOFC/COFC
100	100	Outside Dimensions	TOFC/COFC

The illustration changed the following:

- Nominal Capacity—Line 3, Cols. 24-26.
- Outside Length—Line 2, Cols. 20-24.
- King Pin Setting—Line 4, Cols. 46-47.
- Fitting Code (removed)—Line 4, Col. 28.

NOTE 1: To remove data from a field, leave the data change field blank.
 NOTE 2: When reporting numeric fields leading zeroes must be reported.

1-4	3-6	Unit Initials	TOFC/COFC
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Report the alphabetic initials or marks stenciled on unit. Do not report special characters or punctuation, e.g., B&OZ report as BOZ. Non-standard initials see Exhibit N.

1-4	7-12	Low Unit Number	TOFC/COFC
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Lowest unit number of a consecutively numbered homogenous group of identical units. Do not use special characters or punctuation. Units stenciled with more than six (6) numeric positions; report the first six (6) numbers. Non-standard numbers see Exhibit N.

EXAMPLE: Unit number is 123, report 000123.

NOTE: For Railroad owned/controlled units, the numbers must be stenciled in accordance with Section 3, Rule 1 (a), (b), (c), of the Code of Trailer and Container Service Rules. This rule applies only to Z0____ and Z2____ thru Z9____ built after December 1983; and Z1____ and U____ built after December 1980.

1-4	13-18	High Unit Number	TOFC/COFC
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Mandatory except for single units. Report high number of group above, e.g., unit number 1622 report as 001622. If this field is left blank, it will be assumed equal to the low number.
 NOTE: No more than 1500 units can be reported in a series. Non-standard numbers see Exhibit N.

NOTE: For Railroad owned/controlled units, the numbers must be stenciled in accordance with Section 3, Rule 1 (a), (b), (c), of the Code of Trailer and Container Service Rules. This rule applies only to Z0____ and Z2____ thru Z9____ built after December 1983; and Z1____ and U____ built after December 1980.

1	19-22	Unit Type Code	TOFC/COFC
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Four (4) position alphanumeric code giving a general physical description of the type of unit. (See Exhibit D for the appropriate code). Code must agree relationally with the number as prescribed in Trailer Service Rule 1, Section 3. Errors will cause the transaction to repeat.

1	23-26	Owner's Mark	TOFC/COFC
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1. For Railroad owned equipment having railroad reporting marks report the major uniform alphabetic code of the railroad owning the equipment. This code must never end with the letters U or Z.

2. For privately owned equipment report the major uniform alphabetic code of the private company owning the equipment.

This field must be completed. Failure to provide the appropriate owner's mark will result in the rejection of the transaction.

NOTE: This field is used to count ownership for Car Service reports when the lessee field is blank and is also used to access car location tracing through TRAIN inquiries. This field must be kept up-to-date!

1	27-30	Lessee's Mark	TOFC/COFC
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Report the uniform alphabetic code of railroad or private owner leasing unit. This field may be left blank, however, if reported, the reporting mark cannot be equal to the owner's field or be a family reporting mark. Units with hybrid reporting marks must have the railroad's major reporting mark, e.g., RLNZ report SBD.

NOTE: This field is used to count ownership for Car Service reports when the lessee field is blank and is also used to access car location tracing through TRAIN inquiries. This field must be kept up-to-date!

1	31-32	Month Built	TOFC/COFC
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Numeric, report the month the unit was built or rebuilt, e.g., 01 = January.

1	33-34	Year Built New	TOFC/COFC
---	-------	----------------	-----------

Last two (2) digits of year built, e.g., year built 1981, report 81 in this field. This field must never be blank.

1	35-36	Year Rebuilt	TOFC/COFC
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Last two (2) digits of year rebuilt. The rebuilt year cannot be equal to the year built new. If not applicable report zeros.

- Applicable to units rebuilt between July 1, 1976 through March 1, 1981 inclusive that comply with all DOT regulations.
- Units rebuilt subsequent to March 1, 1981, must comply with AAR, Mechanical Division specifications.

1	37-38	Year 2nd Hand	TOFC/COFC
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Last two (2) digits of year acquired (denotes change in ownership). Year built new or rebuilt must also be reported. (Cols. 33-34).

1	39-42	Advance Installation Date	TOFC/COFC
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Month and year unit is to be installed into service, report month in Cols. 39-40, year in Cols. 41-42, e.g., October 1983 report 1083. Do not report the current month and year unless it is the installation date. If left blank or date less than current is provided the current date will be inserted.

Units may be advance registered up to one (1) year, e.g., in January 1983 the maximum advance registration date will be 0184. Question: Do expired installation dates have to be re-advanced—Yes! Question: If a unit is placed into interchange before the advanced installation date do I have to submit an update—Yes! Question: Is the effective date of the rate or the advanced installation date in the output tape? Answer—The advance installation date.

1	43-48	Original Cost (\$)	TOFC/COFC
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For railroad-owned units, report in U.S. dollars the original ledger value of original owner as prescribed in ICC Accounting Rules. (Exclude additions and betterments and refrigeration units). This field may be left blank for privately-owned units.

NOTE: Raise all cents to the next dollar, e.g., \$5501.02 = 005502.

UMLR SPECIFICATION MANUAL—SECTION III—CONTINUED

1 49 Indicator for Positive/Negative A&B (RR) TOFC/COFC
This field must be reported when reporting additions or betterments Line 1, Cols. 50-54, otherwise report blank.

P—Indicates the addition of a capitalized cost of additions or betterments made to a unit subsequent to its installation into service.

N—Indicates the subtraction of a capitalized cost of additions or betterments that were removed from the unit subsequent to its installation into service.

1 50-54 Additions & Betterments (RR) TOFC/COFC

1. For railroad-owned units if applicable, report in U.S. dollars the cost for the following:
 - a. Additions are new parts applied subsequent to the date the unit was built and are carried in the capital investment account.
 - b. Betterments are improvements of parts of existing equipment through the substitution of superior parts for inferior parts replaced.
2. This field may be left blank for privately-owned units.

NOTE: Raise all cents to the next dollar, e.g., \$5501.02 = 05502.

MINIMUM—00000 MAXIMUM—

1 55-60 Ledger Value TOFC/COFC

1. Report in U.S. dollars the total of the original cost, plus applicable additions and betterments (Cols. 49-54) as carried in the equipment valuation accounts. Do not report the cost for the refrigeration unit.
2. This field may be left blank for private equipment.

NOTE: Raise all cents to the next dollar, e.g., \$5,501.02 = 05502.

MINIMUM—000100 MAXIMUM—

1 61 Indicator TOFC/COFC

A one position Alpha/Numeric field:

0—Unit in error (Inserted by AAR).

1—Units subject to special lease arrangement.

NOTE: Once the indicator has been changed to zero, the owner must reinstate the appropriate indicator code.

1 62-80 Blank TOFC/COFC

Line 2, Col. 1 must be 2. Cols. 2-18 must equal Line 1.

2 19 Frame Type COFC

** Alphabetic, report the 1 character field containing a code to describe the type of frame enclosing the tank container.

This field enables the identification of the type of vehicle needed to handle the container for loading onto and removal from the conveyance vehicle.

Unit can be lifted from the center: Yes=Y, No=N

2 20-24 Outside Length TOFC/COFC

Measurement of overall length of trailer or container. Must agree with the TOFC/COFC Type Code. Must be greater than the Inside Length reported. Feet in Cols. 20-22, inches in Cols. 23-24. Round fraction to the higher inch, e.g., 04 1/8" = 05.

Trailers: MINIMUM—015 07 MAXIMUM—048 00

Containers: MINIMUM—005 11 MAXIMUM—048 00

2 25-28 Outside or Extreme Width TOFC/COFC

Measurement of the outside width of the unit including attachments projecting to greatest extent. Must agree with the TOFC/COFC Type Code. Must be greater than the Inside Width data reported. Feet in Cols. 25-26, inches in Cols. 27-28. Round fraction to the higher inch, e.g., 5 1/8" = 06.

Trailers: MINIMUM—07 04 MAXIMUM—08 06

Containers: MINIMUM—04 06 MAXIMUM—08 06

2 29-32 Height of Extreme Width TOFC/COFC

Measurement is from ground to the highest point where the Extreme Width occurs. Must not exceed the Extreme Height data reported. Feet in Cols. 29-30, inches in Cols. 31-32. Round fraction to the higher inch, e.g., 05 1/8" = 06.

Trailers: MINIMUM—01 00 MAXIMUM—14 00

Containers: MINIMUM—00 08 MAXIMUM—13 06

2 33-36 Outside or Extreme Height TOFC/COFC

Measurement from ground to extreme projecting height. Must agree relationally with TOFC/COFC Type Code, and must be greater than the Inside Height data reported. Feet in Cols. 33-34, inches in Cols. 35-36. Round fraction to the higher inch, e.g., 05 1/8" = 06.

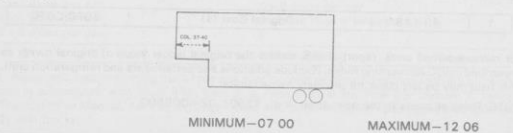
Trailers: MINIMUM—04 04 MAXIMUM—14 00

Containers (Excluding U6): MINIMUM—01 00 MAXIMUM—13 06

Tank Containers: MINIMUM—04 00 MAXIMUM—08 06

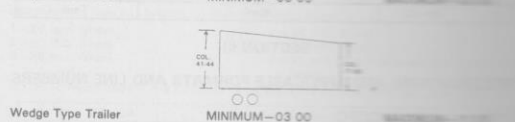
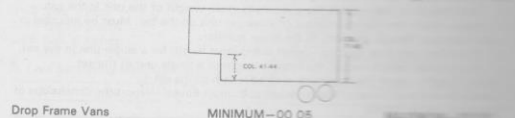
2 37-40 Length of the Upper Step TOFC/COFC

Measurement of the length of the van ahead-of-the-drop. For Z8 types only. (See diagram below). Feet in Cols. 37-38, inches in Cols. 39-40. Round fraction to the lower inch, e.g., 05 3/4" = 05.



2 41-44 Height of the Step or Wedge TOFC/COFC

Measurement of the drop depth for drop frame vans (Z8 types). Measure from the top of the height at the rear of the wedge type trailers (Z6 types). (See diagram below). Feet in Cols. 41-42, inches in Cols. 43-44. Round fraction to the lower inch, e.g., 05 3/4" = 05.



2 45-48 CTC Special Permit Number COFC

** Numeric, report the four position number indicating the approval permit number issued by the Canadian Transport Commission, e.g., No. 730 report 0730. Report zeros.

2 49 FRA Approval COFC

** Alphabetic, report the applicable code identifying the type of service for which the container has been approved.

B—TOFC and COFC Service

C—COFC Service

T—TOFC Service

X—DOT/FRA

2 50 AAR.600 Approval COFC

** Alphabetic, report one character field used to define the type of service for which the container has been approved for by AAR.600.

H—Approved for Hazardous Materials Handling

N—Not Approved for Hazardous Materials Handling

X—Not DOT/FRA Approved

2 51 Side Door Type TOFC/COFC

Numeric code to indicate type of side door. Report zero unless Line 2, Col. 51 is reported, or if unit is a flat type, bulk hopper, tank or chassis. Use one of the following codes:

- | | |
|-----------------------------------|---|
| 0—No side door or doors | 4—Roadside Overhead/Roll-up door |
| 1—Curbside leaf door or doors | 5—Both Curbside/Roadside leaf doors |
| 2—Roadside leaf door or doors | 6—Both Curbside/Roadside Overhead/Roll-up doors |
| 3—Curbside Overhead/Roll-up doors | 7—Other type side doors |

2 52 End Door Type TOFC/COFC

Numeric code to indicate type of end door. Report zero unless Line 2, Col. 52 is reported, or if unit is a flat type, bulk hopper, tank, or chassis. Use one of the following codes:

- | | |
|--------------------------|-------------------------|
| 0—No end door or doors | 2—Overhead/Roll-up door |
| 1—Leaf end door or doors | 3—Other type end door |

2 53-56 Side Door Width TOFC/COFC

Width of side door openings in the clear. If more than one opening on the side, report the width of the maximum continuous opening. If no openings or doors, or permanently closed doors, report zeroes in Side Door Type field, Line 2, Col. 51. Feet in Cols. 53-54, inches in Cols. 55-56. Round fraction to the lower inch, e.g., 05 3/4" = 05.

Trailers & Containers MINIMUM—03 00

NOTE: For chassis, flats, bulk hoppers and tanks this field is zero filled.

2 57-60 Side Door Height TOFC/COFC

Height of side door openings in the clear. Blank if Line 2, Cols. 53-56 are blank. If no openings or doors, or permanently closed doors, report zeroes in Side Door Type field, Line 2, Col. 51. Feet in Cols. 57-58, inches in Cols. 59-60. Round fraction to the lower inch, e.g., 05 3/4" = 05.

Trailers & Containers MINIMUM—06 00

NOTE: For chassis, flats, bulk hoppers and tanks this field is zero filled.

2 61-64 End Door Width TOFC/COFC

The width of the operating end door opening in the clear. Feet in Cols. 61-62, inches in Cols. 63-64. Round fraction to the lower inch, e.g., 05 3/4" = 05. If no door, or door is inoperable, report zeroes in End Door Type field Line 2, Col. 52. This field is zero filled for chassis, flat, bulk hopper and tank.

Trailers: MINIMUM—04 00

Containers: MINIMUM—03 00

2 65-68 End Door Height TOFC/COFC

The height of the operating end door opening in the clear. Report feet in Cols. 65-66, inches in Cols. 67-68. Round fraction to the lower inch, e.g., 05 3/4" = 05. If no door, or door is inoperable, report zeroes. This field is zero filled for chassis, flat, bulk hopper and tank.

Trailers: MINIMUM—04 00

Containers: MINIMUM—05 00

UMLER SPECIFICATION MANUAL—SECTION III—CONTINUED

69-72	Inside Length	TOFC/COFC
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ment to be taken from end to end between linings for van, open top and refrigerator. Types, measurement to be that of the loading platform (usable space). If unit equipped with bulkheads, measure distance between bulkheads. *This field is blank for tanks.* Data must be less than outside length data. Feet in Cols. 69-70, inches in Cols. 71-72. Round fraction to nearest inch, e.g., 05 3/4" = 05.

Report: MINIMUM—15 06 MAXIMUM—47 10
Inches: MINIMUM—05 00 MAXIMUM—47 10

73-76	Inside Width	TOFC/COFC
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of the interior of the van, open top or refrigerator between linings. Where sides are inset, measurement is at the narrowest point between belt rails, nailing girth or ventilating strips. For flat types, measurement is width of the loading platform. *This field is blank for tanks.* Data must be less than outside or extreme width data. Feet in Cols. 73-74, inches in Cols. 75-76. Round fraction to the lower inch, e.g., 05 3/4" = 05.

Report: MINIMUM—07 00 MAXIMUM—08 04
Inches: MINIMUM—04 00 MAXIMUM—08 04

77-80	Inside Height	TOFC/COFC
-------	---------------	-----------

For Tank & Bulk Hopper: For van, open top or refrigerator measurement is taken from floor of sides of unit. For flat types measurement is taken from ground to loading platform. Data must be less than the outside, or extreme height data. Feet in Cols. 77-78, inches in Cols. 79-80. Round fraction to the lower inch, e.g., 05 3/4" = 05. *This field is blank for tank and bulk hoppers.*

Report (Except Flats): MINIMUM—03 00 MAXIMUM—11 01
Inches (Flats): MINIMUM—01 00 MAXIMUM—07 11
Inches (Except Flats): MINIMUM—05 10 MAXIMUM—12 06
Inches (Flats): MINIMUM—01 00 MAXIMUM—06 11

Col. 1 must be 3. Cols. 2-18 must equal Lines 1 and 2.

19-23	Cu. Ft./Gallone Cap.	TOFC/COFC
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Inside volume of trailer or container in cubic feet—end to end, side to side, and from top of sides. For tanks report actual gallonage capacity.

Report: MINIMUM MAXIMUM
Open Top, Refrig., Bulk Hopper Trailers—01000 Trailers—03700
Containers—00200 Containers—04500
Flat, Flatbed & Extendable—ZERO FILLED Trailers—04000 Trailers—12000
Containers—01500 Containers—09000

24-26	Blank	TOFC/COFC
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27-30	Tare Weight (00)	TOFC/COFC
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Report weight, or empty weight, in hundreds of pounds, of the trailer or container, as required by AAR TOFC Interchange Rules, Section O, Rule 172.

Reporting instructions: If units and tens position of actual empty weight is 50 or less, round to next lowest 100. If units and tens position is 51 or greater, round to next highest 100. Actual 7500, report 0075; actual 7551, report 0076.

Report: MINIMUM—0060 MAXIMUM—0230
Inches: MINIMUM—0006 MAXIMUM—0190

31-33	Tank Material Specification	COFC
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Report numeric, report the American Society for Testing Materials (ASTM) specification or, describing the material used in construction of the tank. If the tank material specification is identified by a foreign country's specification report the specification. Otherwise, the specification will be one of the following:

Report: A01 C-01 = C01 S-01 = S01

34	Insulation Material	COFC
----	---------------------	------

Report alphabetic, report the code to identify the type of insulation material.

Report: Rk

Report: Rm

Report: Rgl

Report: Rg

Report: Rg

Report: Rg

Report: Rg

Report: Rg

Report: Rg

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3	41-48	Blank	COFC
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3	41-48	Tire Size & Wheel Size	TOFC
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Numeric representation of the tire size, to be reported for Z types only. Do not use special characters or punctuation, i.e., 10:00x20, report as 10002000 or 11:00x22.5 report as 11002250.

MINIMUM—07351400 MAXIMUM—11002400

3	49-62	Blank	TOFC
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3	49-62	Head Thickness	COFC
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* Numeric, report the 4 position numeric field "0.000" in decimal inches defining the thickness of the head material, e.g., report 0.125 as 0125. If less than 0.125 the AAR.600 code (Line 2, Col. 50) will be 'X'.

MINIMUM—0100 MAXIMUM—1250

3	53-66	Shell Thickness	COFC
---	-------	-----------------	------

* Numeric, report the 4 position numeric field "0.000" in decimal inches defining the actual thickness of the shell material report 0.125 as 0125. If less than 0.125 the AAR.600 code (Line 2, Col. 50) will be 'X'. The reported head thickness cannot be thinner than the shell thickness.

MINIMUM—0100 MAXIMUM—1250

3	57-69	Max. Allowable Working Pressure	COFC
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* Numeric, report maximum pressure in pounds per square inch (PSI) that the container was designed to be operated. If the minimum (PSI) is less than 035 the AAR.600 code (Line 2, Col. 50) will be 'X'.

MINIMUM—020 MAXIMUM—500

3	60-62	Start-to-Discharge Pressure	COFC
---	-------	-----------------------------	------

* Numeric, report the maximum pressure in pounds per square inch (PSI) the container will contain before its start-to-discharge point will be reached. This information allows an accurate assessment of the condition of a tank if it is in danger. If the minimum (PSI) is less than 035 the AAR.600 code (Line 2, Col. 50) will be 'X'.

MINIMUM—020 MAXIMUM—500

3	63-66	Gross Weight (00)	COFC
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* Numeric, report the maximum allowable gross weight of the loaded container in hundreds of pounds. This information relates to crane lifting capacity for loading and unloading purposes, the weight distribution on the flat car, and the weight limitation of flat cars and of chassis. Providing this data through the computer will simplify the loading process and act as a double check to ensure that placement is proper.

MINIMUM—0043 MAXIMUM—0750

3	67-70	Load Limit (00)	TOFC/COFC
---	-------	-----------------	-----------

* Numeric, report in hundreds of pounds the maximum allowable weight of the loading. The load limit is the difference between the stencilled gross weight and tare weight.

MINIMUM—0043 MAXIMUM—0695

3	* 71-80	Blank	TOFC/COFC
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Line 4, Col. 1 must be 4. Cols. 2-18 must equal Lines 1, 2 and 3.

4	19	Transportation Code	TOFC/COFC
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Alphabetic code indicating type of service or loading restriction of unit.

S—Condemned unit or one designated for scrap or dismantling. Automatically overrides all other codes.

X—Unit is prohibited or restricted in interchange service by AAR Interchange Rules.

Automatically overrides all other codes, except S.

G—Unit is used for the loading of contaminated commodities.

A—Unit covered by Trailer Service Rules.

M—Reporting Mark cancelled by AAR.

O—Unit must be returned to owner.

4	20	Transportation Condition Code	TOFC/COFC
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Alphabetic code describing the handling of unit.

A—Unit designated a general service by owner must be handled in accordance with Trailer Service Rule 1.

B—Unit not designated a general service by owner must be handled in accordance with Trailer Service Rule 2.

4	21-23	Notes	TOFC/COFC
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Report any numeric note references displayed in *The Official Intermodal Equipment Register*. NOTE: If up to three notes are applicable, and the note references are limited to 1 character each, start in the high order position (Line 4, Col. 21) with first reference and continue through the other two columns.

e.g., SFTZ 500019-500020 shows note references 1,2,3.

Report: 1 2 3

In Columns: 21 22 23

4	24-33	Fitting Codes	TOFC/COFC
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* Three 2-position codes showing types of equipment installed in the trailer or container, including special devices, components, or appurtenances. May be alphabetic or numeric. If unit type code is Z9, then either one or more of fitting codes BR, BS or DD. FI must be reported for Z7. If U5 or Z5 FS is applicable. See Exhibit G for additional fitting codes applicable to TOFC/COFC equipment.

UMLER SPECIFICATION MANUAL—SECTION III—CONTINUED

Fitting 1—If only one fitting, report in Cols. 24-25.
 Fitting 2—If two fittings, report in Cols. 24-25 and 26-27.
 Fitting 3—If three fittings, report in Cols. 24-25, 26-27 and 28-29.
 Fitting 4—If four fittings, report in Cols. 24-25, 26-27, 28-29 and 30-31.
 Fitting 5—If five fittings, report in Cols. 24-25, 26-27, 28-29, 30-31 and 32-33.

4	34	Safety Devices	COFC
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* Alphanumeric, report the code designed to define safety devices applied to the container, e.g.: safety vent, safety valve, fusible link, or combinations.
 For example, European portable tank containers are permitted to have non-reclosing safety vents in flammable service whereas this is not allowed in American rail service. This is an area of particular concern to the railroads as non-complying tanks can inadvertently be routed by rail. It is also helpful to know what exterior accessories might be involved from which material releases can be expected if the tank is in distress.

- 1-Vent
 2-Valve
 3-Combination Vent & Valve
 4-Fusible Link
 5-Combination Fusible Link & Vent

4	35	Undercarriage	TOFC
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Numeric representation of type tandem of undercarriage.

- 1—Fixed Forward (Eastern Setting)
 2—Fixed Rear (Western Setting)
 3—Sliding Capability
 4—Fixed Forward (Eastern Setting) Width 08'08"
 5—Fixed Rear (Western Setting) Width 08'08"
 6—Sliding Capability, Width 08'08"

4	36	Body Type	TOFC/COFC
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An alphabetic code describing the exterior composition of the unit body, exclusive of the undercarriage or underframe.

- A—Aluminum
 F—Fiberglass, or Fiberglass Reinforced Material
 M—Miscellaneous or other type materials
 S—Standard Steel
 T—Stainless Steel
 W—Wood
 E—Combination

If the body is constructed of a mixture or combination of these materials, then the symbol E must appear. NOTE: If Chassis or flat type, this field is blank.

4	37	Floor Type	TOFC/COFC
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Alphabetic or numeric code indicating type or composition of the floor. See Exhibit H for list of applicable codes.

If numeric: MINIMUM—1 MAXIMUM—9
 If alpha: Must be A, C, D, N, P, S, W or X.
 NOTE: Blank for bulk hoppers, chassis and tanks.

4	38	Valve Type	COFC
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* Alphanumeric, report the 1 character field containing a code identifying the type of bottom outlet applied to the container.

- 1—No Valve
 2—Internal Valve with 2 consecutive closures
 3—Internal Valve with 3 consecutive closures
 4—External Valve with 2 consecutive closures
 5—External Valve with 3 consecutive closures

4	39	Lining	TOFC
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Alphabetic code to show the type of lining used in the unit. See Exhibit E for list of codes. For chassis, flats, flatbeds and extendables, field must be blank.

4	39	Lining	COFC
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* Alphabetic, report the code defining the type of material used to line the container. Report lining material. BLANK FOR UNEQUIPPED.

- B—Nickel electro-plating, e.g., Bart
 G—Glass (any thickness)
 K—Electroless plating, e.g., Kanigen.
 L—Lead lined (any thickness)
 R—Rubber (applied in sheet form). Carbon Steel tanks only.
 S—Stainless cladding
 T—Paint, Phenolic, Epoxy and metal spray deposits such as Lithocote, Amercoat, Hersite, PPG Columbia #7 Neoprene Rubber Latex
 V—Poly-Vinyl Chloride

4	40	Roof Type	TOFC/COFC
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A one position numeric code showing roof characteristics of the unit. This field may be left blank, and is blank for flat types and tanks. Use one of the following codes:

- 1—Trough Hatch in Roof
 2—Removable Roof
 3—Self-Storing Roof
 4—Standard Roof Hatch
 5—Insulated
 6—Open Top (Including Bows & Tarps)
 7—Not Used
 8—Not Used
 9—Not Otherwise Listed by Type

4	41	Outlets	TOFC/COFC
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Numeric code 1 thru 6 indicating the number of discharge outlets in the bottom of a bulk hopper or tank trailer or container (TOFC/COFC Type Codes Z0 or U0). Must be at least a one (1).

- 1 = 1 outlet
 2 = 2 outlets
 3 = 3 outlets
 4 = 4 outlets
 5 = 5 outlets
 6 = 6 outlets

4	42	Compartments	TOFC/COFC
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* Numeric code 1 thru 6 to indicate the number of compartments or bays in a bulk hopper or tank trailer or container (TOFC/COFC Type Codes Z0 or U0, Z6 or U6). Must be at least a one (1).

- 1 = 1 compartment
 2 = 2 compartments
 3 = 3 compartments
 4 = 4 compartments
 5 = 5 compartments
 6 = 6 compartments

4	43	Type Brakes	TOFC/COFC
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An alphabetic code indicating the braking system.

- A—Air
 E—Electric
 V—Vacuum

4	44	Wheel Type	TOFC
---	----	------------	------

An alphabetic code indicating type of wheels.

- B—Budd
 D—Dayton
 M—Miscellaneous or other

4	45	Axles	TOFC
---	----	-------	------

A numeric code indicating number of axles.

- 1—Single Axle
 2—Tandem Axles
 3—Triple Axles

4	46-47	King Pin Setting	TOFC
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Numeric representation in inches. Must be one of the following: 18, 28, 30, 32 or 36 (standard).

4	48-49	Licensing State	TOFC/COFC
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An alphabetic code representing the Standard Alphabetic Abbreviation for state, province or foreign country used to designate the prime or home licensing location. May be blank unless one of the applicable codes in Exhibit I.

4	50-52	Container Corner Casting	COFC
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Alphabetic code to indicate the tie-down, handling, and stacking ability for containers. Use one of the following codes:

USA—Includes ASA and ANSI. Oval bottom opening, 9/16" bottom wall. (ASA—American Standard Association. ANSI—American National Standards Institute).

ISO—ISO type only. Oval bottom opening, 1-1/8" bottom wall. (ISO—International Organization for Standardization).

SEA—Sea-Land only—unique.

MAT—Matson only. Round bottom openings.

OTH—Other designs of corner castings.

4	53	Type of Controlled Atmosphere	TOFC/COFC
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Alphabetic code to indicate type of controlled atmosphere system. Blank unless TOFC/COFC Type Code is Z5 or U5. May be blank or use one of the following codes:

- O—Oxytrol
 T—Tectrol
 U—Other Type System

4	54	Refrigeration Unit Location	TOFC/COFC
---	----	-----------------------------	-----------

Alphabetic code to indicate the location of the refrigeration unit. Blank unless Line 4, Col. 53 is reported, then report one of the following codes:

- S—Side Mounting
 U—Under or Belly Mounting
 N—Nose or Front Mounting
 P—"POD" Mounting (Nose Evaporator, See Condensor)

4	55-58	Refrigerator Fuel Capacity	TOFC/COFC
---	-------	----------------------------	-----------

Numeric representation of the capacity of the fuel tank of the refrigeration unit. Report the capacity in actual gallons or pounds, as commonly expressed.

- If gallons: MINIMUM—0030 MAXIMUM—0250
 If pounds: MINIMUM—0010 MAXIMUM—0250

4	59	Refrigerator System Manufacturer	TOFC/COFC
---	----	----------------------------------	-----------

Alphabetic code to indicate make of the mechanical refrigeration system. Blank unless equipped with a refrigeration or heating system, then may be blank or one of the following:

- C—Carrier-Transcold
 F—Trans-Artic Traveler
 P—Polarstream
 T—Thermo-King
 W—Worthington-York
 M—Other Manufacturers

4	60	Type of Protective Fuel	TOFC/COFC
---	----	-------------------------	-----------

Alphabetic code to indicate type of fuel required to sustain the protective service. Blank unless Col. 59, Line 4 reported, or unless trailer or container equipped with refrigeration or heating system.

- B—Butane
 D—Diesel
 G—Gasoline
 N—Nitrogen
 P—Propane
 M—Other type

4	61-62	Electrical Voltage System	TOFC/COFC
---	-------	---------------------------	-----------

Numeric code to indicate the voltage required by the trailer or container for central power in Perishable Protective Service. For Z5, Z7, Z9, U5, U7 and U9 types report one of the following:

- 06 = 06 Volts
 11 = 110 Volts
 12 = 12 Volts
 22 = 220 Volts
 24 = 24 Volts
 33 = 330 Volts
 44 = 440 Volts
 00 = Unused or restricted

4	63-67	DOT Exemption	COFC
---	-------	---------------	------

* Numeric, report the Exemption Number e.g., No. 870 applicable to high pressure vessels follows: 00870. If not applicable report zeros.

4	67-80	Blank	TOFC/COFC
---	-------	-------	-----------

UMLER SPECIFICATION MANUAL—SECTION IV

SECTION IV
EFFECTIVE OCTOBER 1, 1983
SPECIFICATIONS AND APPLICABLE FORMATS AND LINE NUMBERSTYPE: LOCOMOTIVE
FORMAT: E
LINE: 1, 2, 3, 4

Line	Column No.	Field Description	Locomotive
------	------------	-------------------	------------

NOTE: Columns 2 through 18 are repeated on Lines, 2 through 4.

1-4	1	Appropriate Line Number	Locomotive
-----	---	-------------------------	------------

1-4	2	Transaction Code	Locomotive
-----	---	------------------	------------

To DELETE a locomotive from the file, the transaction code must be according to Exhibit C. There are three methods that can be used: 1) submit the entire first line of information; 2) the complete tape record; or 3) submit UMLER Deletion Format (Exhibit C), e.g., (1.) for single locomotive submit the appropriate delete code, locomotive initials, low number (high number may be left blank), and applicable AAR car type code or four zeroes, (b) for locomotive in consecutive numbered series do not report the applicable AAR car type code; report the arithmetic difference between the low and high number.

To ADD a locomotive or consecutively numbered series to the file, the transaction code must be a two (2).

A change in locomotive initials and/or numbers requires a complete ADD registration (transaction Code 2) under the new initials and numbers and a DELETE (Transaction Code A) of the former initials and numbers.

To CHANGE data (other than locomotive initials and number) on the file, the transaction code must be a three (3). One of two methods can be used:

1. Standard Method: Submit code three (3) in each complete line of information being submitted or the complete tape record.

2. Select Field Method: Transaction code must be a four (4). 1. This method can be used to change as many as five data fields for a single locomotive or consecutively numbered locomotive series. 2. On-line (TCAM) submitters can report up to ten (10) fields of information. See Train Manual, UMLER section V, page 27). The Select Field Form (Exhibit B) must be used.

- Report locomotive initials.
- Report low number and the high number for consecutively numbered locomotive series.
- Report car number in which data change is to be made.
- Report field identity number, i.e., the first column in which data are reported.
- Report data to be changed.

NOTE: To remove data from a field, leave the data change field blank.

1-4	3-6	* Unit Initials	Locomotive
-----	-----	-----------------	------------

Report the alphabetic initials or marks stenciled on unit. Do not report special characters or punctuation, e.g., B&O report as BO. Numbers duplicating freight equipment cannot be reported.

1-4	7-12	* Low Unit Number	Locomotive
-----	------	-------------------	------------

Numeric, lowest unit number of a consecutively numbered group of identical locomotives. Do not report alphabetic characters.

EXAMPLE: Locomotive number 123, Report 000123.

1-4	13-18	* High Unit Number	Locomotive
-----	-------	--------------------	------------

Numeric, report high number of group above, e.g., Locomotive Number 125 report as 000125. If this field is left blank, it will be assumed equal to the low number. Do not report alphabetic characters.

Note: No more than 1500 locomotives can be reported in a series.

1	19-22	* Unit Type Code	Locomotive
---	-------	------------------	------------

Four (4) position alphanumeric code giving a general physical description of the type of locomotive. (See Exhibit D for the appropriate code).

1	23-26	* Owner's Mark	Locomotive
---	-------	----------------	------------

Report either the uniform alphabetic code of the railroad owning or leasing locomotive or the assigned reporting mark of the private car company owning or leasing the locomotive. DO NOT SHOW BANKS OR EQUIPMENT TRUST HOLDERS.

THIS FIELD MUST BE COMPLETED. Failure to provide the appropriate owner's mark will result in the rejection of the transaction.

1	27-30	Lessee's Mark	Locomotive
---	-------	---------------	------------

Report the uniform alphabetic code of railroad. This field may be left blank, if reported, the reporting mark cannot be equal to the owner's field or be a family reporting mark.

1	31-32	* Month Built	Locomotive
---	-------	---------------	------------

Numeric, report month the locomotive was built or rebuilt, e.g., 01 = January.

1	33-34	* Year Built New	Locomotive
---	-------	------------------	------------

Numeric, last two (2) digits of year built, e.g., year built 1983, report 83 in this field. This field must never be blank.

1	35-36	Year Rebuilt	Locomotive
---	-------	--------------	------------

Numeric, last two (2) digits of year rebuilt. * Cannot be equal to the year built new.

1	37-38	Year Second Hand	Locomotive
---	-------	------------------	------------

Numeric, last two (2) digits of year acquired (denotes change in ownership). Year built new, and rebuilt, if applicable, must also be reported.

1	39-42	Advanced Installation Date	Locomotive
---	-------	----------------------------	------------

Numeric, month and year locomotive is to be installed into service. Report month in Columns 39-40, year in Columns 41-42, e.g., July 1983 Report 0783. Do not report the current month and year unless it is the installation date. If left blank, or date less than current is provided, the current date will be inserted by the AAR.

Locomotive may be advanced registered up to one (1) year, e.g., in January 1983 the maximum advance registration date will be 0184. Question: Do expired installation dated have to be readvanced—Yes!

1	43-60	Blank	Locomotive
---	-------	-------	------------

1	61	Rate Indicator	Locomotive
---	----	----------------	------------

A one position alphanumeric code
6—Unit subject only to contract rate

1	62-80	Blank	Locomotive
---	-------	-------	------------

Line 2 Col. 1 must be a 2, Cols. 2-18 must equal Line 1.

2	19	Clearance	Locomotive
---	----	-----------	------------

Alphabetic, code to indicate clearance of locomotive. See Exhibit F.

Report B—if clearance does not exceed Plate B.

Report C—if clearance is greater than Plate B, but does not exceed Plate C.

Report E—if clearance is greater than Plate B and C, but does not exceed Plate E.

Report F—if clearance is greater than Plate B, C and E, but does not exceed Plate F.

Report G—if clearance is greater than Plates B, C, E and F.

2	20-24	* Outside Length	Locomotive
---	-------	------------------	------------

Numeric, distance over pulling faces of couplers in normal positions. Feet in columns 20-22, inches in columns 23-24. Round fraction to the higher inch, e.g., 05 1/4" = 06.

2	25-28	* Maximum Width	Locomotive
---	-------	-----------------	------------

Numeric, report feet and inches. Feet in Cols. 25-26, inches in Cols. 27-28. Round fraction to higher inch, e.g., 05 1/4" = 06.

2	29-32	Maximum Height From Rail *	Locomotive
---	-------	----------------------------	------------

Numeric, report feet and inches from top of rail to major appurtenance. Feet in Cols. 29-30, inches in Cols. 31-32. Round fraction to higher inch, e.g., 05 1/4" = 06.

2	33-34	Minimum Curvature *	Locomotive
---	-------	---------------------	------------

Numeric, report degrees coupled minimum curvature. 0 (degrees)

2	35-36	Coupled Minimum Curvature *	Locomotive
---	-------	-----------------------------	------------

Numeric, report degrees coupled minimum curvature. 0 (degrees)

2	37-38	Sand Capacity	Locomotive
---	-------	---------------	------------

Numeric, report in cubic feet the sand box capacity.

2	39-41	* Minimum Continuous Speed	Locomotive
---	-------	----------------------------	------------

* Numeric, report the minimum speed in miles per hour, e.g. 10 report 010. For car type code D6 __ report zero (0).

2	42-44	* Maximum Speed	Locomotive
---	-------	-----------------	------------

* Numeric, report the maximum speed in miles per hour, e.g. 90 report 090. For car type code D6 __ report zero (0).

2	45-50	* Weight on Drivers	Locomotive
---	-------	---------------------	------------

Numeric, report in pounds.

2	51-54	* Horsepower	Locomotive
---	-------	--------------	------------

* Numeric, showing actual horsepower of unit. For car type code D6 __ report zero (0).

2	55-58	Gear Ratio	Locomotive
---	-------	------------	------------

Numeric, report the actual gear ratio, e.g., 62:15 report 6215.

2	59-62	* Fuel Tank Capacity	Locomotive
---	-------	----------------------	------------

* Numeric, report the actual capacity of fuel tank in U.S. gallons. For car type code D6 __ report zero (0).

2	63-65	Maximum Dynamic Braking	Locomotive
---	-------	-------------------------	------------

Numeric, three position maximum dynamic braking effort.

First digit = lbs x 10,000

Second digit = lbs x 1,000

Third digit = lbs x 100

UMLR SPECIFICATION MANUAL—SECTION IV—CONTINUED

2	66-67	Wheel Size	Locomotive
Numeric, actual diameter of wheels in inches, e.g., 36 inches=36. MINIMUM—36 MAXIMUM—42			
2	68-70	Traction Motor Current	Locomotive
Numeric, first digit=amps x 1000 second digit=amps x 100 third digit=amps x 10			
2	71-73	Blank	Locomotive
2	74	Hood Type	Locomotive
Alphabetic, report one of the below: L—Long Hood S—Short Hood N—None			
2	75-79	* Air Brake Model Number	Locomotive
Five (5) position, may be alphabetic and/or numeric showing actual model number, e.g., TY, 6BL, 24RL, 26L, 26NL left justified. Add 'M' to last position if modified.			
2	80	Blank	Locomotive
Line 3, Col. 1 must be a 3, Cols. 2-18 must be equal to Lines 1 and 2			
3	19-30	Blank	Locomotive
3	31	Cab Heaters	Locomotive
Alphabetic, must be at 50 fahrenheit minimum 12—inches above floor to qualify. E—Electric N—Not equipped W—Water Z—Other			
3	32	Snow Plow Equipped	Locomotive
A—Pilot snow plow (A and only) C—Pilot snow plow both ends B—Pilot snow plow (B and only) N—Not equipped			
3	33	Tunnel Modifications	Locomotive
Alphabetic, Y—Yes, unit equipped with tunnel sensor device. N—No, unit not equipped with tunnel sensor device.			
3	34	Radio Manufacturer	Locomotive
Alphabetic, codes to indicate radio manufacturer: G—General Electric N—Not equipped J—Johnson W—Webco-Harmon M—Motorola Z—Other			
3	35	Horn Type	Locomotive
Alphabetic, report one of the below: A—Nathan Z—Other L—Leslie N—Not equipped			
3	36	Speed Tape	Locomotive
Alphabetic, report one of the below: E—Event M—Mechanical N—Not equipped			
3	37	Water Coolers	Locomotive
Alphabetic, code indicating type of water cooler: A—Refrigerated-Non-ice N—Not Equipped B—Ice Cooled Z—Other			
3	38	Fuel Oil Preheater	Locomotive
Alphabetic, report one of the below: Y—Yes, unit so equipped N—No, unit not equipped			
3	39	Automatic Cooling Water Drain	Locomotive
Alphabetic, report one of the below: Y—Yes, unit so equipped N—No, unit not equipped			
3	40	Air Conditioned	Locomotive
Alphabetic, report one of the below: Y—Yes, unit so equipped N—No, unit not so equipped			
3	41	* Air Brake Multi Unit Hookup	Locomotive
Alphabetic, report one of the below: Y—Yes, AAR standard equipped X—Yes, equipped non-AAR standard N—Not equipped			
3	42	Truck Manufacturer	Locomotive
Alphabetic, code indicating manufacturer of truck, report one below: A—Alco F—Fairbanks Morse B—Baldwin G—General Electric E—EMD Z—Other			
3	43	* Dynamic Brakes	Locomotive
Alphabetic, code indicating Dynamic Brakes type, report one of the below: E—Extended Range Tapered N—Not Equipped F—Extended Range Flat S—Standard Flat L—Field Loop T—Standard Tapered			
3	44	Speed Control	Locomotive
H—Hump L—Lead M—Hump & Lead N—Not Equipped P—Hump & Trail R—Lead & Trail S—Lead, Hump & Trail T—Trail Y—Equipped (Not Specified)			

3	45-48	Truck Center Length	Locomotive
Numeric, report feet and inches. Feet in Cols. 45-46, inches in Cols. 47-48. Round fraction to lower inch, e.g., 05 3/4"=05. MINIMUM—17'06" MAXIMUM—55'06"			
3	49	Type Journal Bearing	Locomotive
Alphabetic code indicating type journal bearing, report one of the below: A—AAR M—Magnus B—Brenco T—Timken F—Friction Z—Other H—Hyatt			
3	50	Deadman Control	Locomotive
Alphabetic, code indicating type of deadman control, report one of the below: A—Alerter N—Not Equipped E—Electric Z—Other F—Foot Pedal			
3	51	* Fuel Shut Off Type	Locomotive
Alphabetic, code indicating Fuel Shut Off Type, report one of the below: H—Houston Standard H1020 S—Snyder N—Not Equipped P—Houston Electronic H3000 B—Buckeye Z—Other Type T—Snyder II			
3	52	Spark Arrester Type	Locomotive
Alphabetic, code indicating type of spark arrester, report one of the below: E—EMD T—Touchstone X—Turbocharger H—Harco N—Not Equipped Z—Other			
3	53	Fuel Saver Manufacturer	Locomotive
Alphabetic, code indicating manufacturer, report one of the below: G—Harmon Select-A-Power V—Vapor H—Harmon Set-A-Speed T—Touchstone Z—Other			
3	54	Alignment Control Coupler	Locomotive
Alphabetic, report one of the below: Y—Yes, unit so equipped N—No, unit not so equipped			
3	55	Jumper Cable Connection	Locomotive
Alphabetic, code indicating type jumper cable connection, report one of the below: A—21-12 Cables D—Combination B—27-Pin AAR Z—Other C—27-Pin Non-AAR			
3	56	Warning Headlight	Locomotive
Alphabetic, code indicating type light, report one of the below: O—Oscillating L—Flashing & Strobe N—None R—Rotating Amber M—Other & Strobe Y—Equipped, type unknown S—Strobe T—Rotating & Strobe Z—Other F—Flashing			
3	57	Toilet Type	Locomotive
Alphabetic, code indicating type of toilet, report one of the below: B—Biology flow through P—Plastic Bag C—Chemical Z—Other D—Direct to Ground N—Not equipped I—Incinerator			
3	58	Low Idle	Locomotive
Alphabetic, report one of the below: Y—Yes, unit so equipped N—No, unit not so equipped			
3	59-68	Radio Model Number	Locomotive
Ten (10) position, may be alphabetic and/or numeric, showing actual model number of radio, left justified.			
3	69-80	Blank	Locomotive
Line 4, Col. 1 must be a 4, Cols. 2-18 must equal Lines 1, 2 and 3.			
4	19-23	Blank	Locomotive
4	24-29	Fittings	Locomotive
Three 2-position codes showing types of equipment installed in unit. May be alphabetic or numeric. See Exhibit G for fitting codes.			
4	30-37	* Manufacturer's Model Number	Locomotive
Eight (8) position, may be alphabetic and/or numeric, showing actual model number of unit, left justified.			
4	38-44	Original Cost	Locomotive
Numeric, report in U.S. dollars the original value of unit. Raise all cents to next dollar, e.g., \$5,501.02=0005502.			
4	45	Indicator For Positive/Negative A + B	Locomotive
Alphabetic, report only when reporting additions and betterments Line 4, Cols. 45-52, otherwise leave blank.			

UMLER SPECIFICATION MANUAL—SECTION IV—CONTINUED, SECTION V

P—indicates the addition of a capitalized cost of addition or betterments made to a unit subsequent to its installation into service.

N—indicates the subtraction of a capitalized cost of addition or betterments that were removed from a unit subsequent to its installation into service.

Numeric, report in U.S. Dollars the cost of addition and/or betterment applied to unit subsequent to its installation into service. Raise all cents to next dollar, e.g., \$15,501.02 = 15502.

a. Additions are new parts applied subsequent to the date the unit was built new.

b. Betterments are improvements of parts of existing equipment through the substitution of superior parts for inferior parts replaced.

Numeric, report in U.S. dollars the original cost, plus addition and betterments. Raise all cents to the next dollar, e.g., \$5,501.02 = 0005502.

Numeric, report in U.S. dollars the original cost, plus addition and betterments. Raise all cents to the next dollar, e.g., \$5,501.02 = 0005502.

Numeric, report in U.S. dollars the original cost, plus addition and betterments. Raise all cents to the next dollar, e.g., \$5,501.02 = 0005502.

SECTION V
INTRODUCTION

In this section the Transportation Division, Association of American Railroads, as the administrator of the industry's car management file has provided the time limits and procedures for establishing reporting pools and car assignments.

1. After the shipper's request for a new pool or a modification to an existing pool has been agreed to by the pool operator(s), such notification must be provided in accordance with AAR form 16C-10 within 24 hours as described in the procedures in Item H, Subsection No. 1.

2. After the car owner and pool operator have agreed to place cars into assigned service such assignments must be reported to the AAR within forty-eight hours of the date agreed as described in the procedures in Item H, Subsection No. 2.

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Section V—Specifications for Pool Header and Car Assignments

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ITEM A
CAR SERVICE RULE 16

(A). When specific cars covered by CSD 145, 165 and 435 series are assigned to a shipper for his use, such assignment shall be made subject to the following conditions.

1. Shipper must request in writing of originating road haul carrier(s) assignment at least (10) days before their intended use of a specific number of cars. If originating carrier(s) agrees to such assignment but desires other road haul carriers to participate in assignment it will advise shipper accordingly and will make request on such other carriers. A road haul carrier may assign cars but must have the concurrence of the originating carrier which may or may not be a switching carrier.

2. Pools of assigned cars including cars of ownership other than the originating road haul carriers may be expanded only upon the acceptance by the originating road haul carriers.

3. When cars are assigned in accordance with this Rule, they will remain and be treated as assigned cars until the shipper, originating road haul carrier(s), pool operator or owning railroad serves notice that such assignment is modified or canceled. One day written notice is sufficient.

(B) Assigned cars shall not be held empty enroute to loading point except as follows:

(a) Upon written instructions of assignee.

(b) When assignee cannot immediately accept on arrival at loading point and cars cannot reasonably be held on other tracks at loading points.

(c) For necessary repairs (not cleaning).

(C) Assignment by a carrier of specific cars for the use of a shipper at a particular point must be reported by the Designated Pool Reporter to the Transportation Division of the Association of American Railroads. This reporting must comply with the formats and time limits prescribed in the UMLER Data Specification Manual. The Transportation Division of the Association of American Railroads will maintain a current record of cars assigned and as well as to the roads originating traffic from such assignment, including originating switching line serving the shipper involved. The foregoing provisions of this paragraph shall not apply when all cars assigned to the use of a shipper at a particular point are system cars of a single road haul carrier serving the shipper at such point.

(D) Assigned cars are exempt from Car Service Rules 1 and 2.
Note: Per new Car Service Rules adopted July 1, 1981 paragraph D, Car Service Rule 16 is also applicable to Car Service Rules 3 and 4, and Car Service Rules 1 and 2 as revised effective July 1, 1981.

ITEM B
TRANSPORTATION DIVISION, CAR SERVICE
DIRECTIVES 145, 155, 165 AND 435

ASSESSMENT ORDER

September 17, 1984
File: 580-145

(REVISED)—CAR SERVICE DIRECTIVE NO. 145

(Cancels and Supersedes CSD 145 of November 15, 1982)

To: Transportation Officers—All Railroads

NOTICE: This Directive is issued under provisions of paragraph (g) Car Hire Rule 19. Each violation of paragraph 2 of the Order Section of this Directive is subject to a graduated scale of assessments, based on violations occurring each calendar month.

EFFECTIVE: 12:01 a.m., October 1, 1984 and continuing until further notice.

APPLICATION: *To cars assigned to shippers or district distribution pools carrying railroad reporting marks of the following mechanical designations: FA, FB, FBS, FC, FL, FMS, GBR, GBS, GBSR, GDS, GSS, GTS, GWS, HMR, HTS, RB, RBL, RPB, RPC, RPL, RPM, XL, XLI, XLH, XP, XPI and all "L" types (except "LO", which are covered by CSD No. 435).

ORDER:

- Waybills accompanying assigned cars under load and station copies, must be endorsed: "Assigned Car CSD No. 145."
- After unloading, cars subject to this Directive must be:
 - Handled per owner's instructions
 - Bill back empty to shipper or agent at loading point in reverse of loaded movement. The holding road to exercise the option.
- Cars doors must be closed and sealed.
- Empty return must be on standard form waybills without charges which must be endorsed "Assigned Car CSD No. 145."
- Switching documents covering loaded or empty car delivered in switching service must be endorsed "Assigned Car CSD No. 145."
- Reclaims for car hire on cars covered by this Directive may be made in accordance with Car Hire Rule 22 and Car Service Rules 1 and 2 are suspended insofar as they may conflict with above provisions.
- No assessment will be made under this order unless:
 - Waybill or switching document is properly endorsed; and
 - Car is listed in UMLER with proper pool assignment.

EXCEPTION: Owners may exempt certain cars from the provisions of paragraph 2 of the Order to allow use by other specified lines, but such exemption must be confirmed in writing to the Transportation Division in advance.

*Reissued to add mechanical designation RPC under APPLICATION section.

By direction of,
J. J. Carroll
Executive Director and Chairman

UMLER SPECIFICATION MANUAL—SECTION V—CONTINUED

July 31, 1981
File: 580-155

(REVISED)—CAR SERVICE DIRECTIVE NO. 155

(Cancels and Supersedes CSD 155 of July 15, 1975)

TO: Transportation Officers—All Railroads and Terminal Switch Lines.

NOTICE: This Directive is issued under provisions of paragraph (g) Car Hire Rule 19. Each violation of Section A.2 or Section B.2 of this Directive is subject to a graduated scale of assessments, based on violations occurring each calendar month.

EFFECTIVE: 12:01 A.M., September 1, 1981 and continuing until further notice.

APPLICATION: To box cars AAR mechanical designation XF, carrying railroad reporting marks.

ORDER:

SECTION A. Cars Assigned to Pool Points.

- Waybills accompanying assigned cars under load, and station copies, must be endorsed "Assigned Car for Processed Packaged Food Loading CSD No. 155."
- After unloading, cars subject to this Directive must be:
 - Handled per owner's instructions.
 - Billed back empty to agent at loading point in reverse of loaded movement.
 The holding road to exercise the option.
- Such empty return must be on standard form waybills without charges which shall be endorsed "Assigned Car for Processed Package Food Loading CSD No. 155."

SECTION B. Unassigned Cars.

- Waybills accompanying unassigned cars under load, and station copies must be endorsed "Unassigned Car for Processed Packaged Food Loading CSD No. 155."
- After unloading, cars subject to this Directive must be:
 - Handled per owner's instructions.
 - Loaded to or via the home road.
 - Returned to the delivering road at the junction where previous load was received if that junction is not a junction with the home road.
 - Returned empty to the delivering road at the junction where previous load was received if that junction is not a junction with the home road.
 The holding road to exercise the option.
- Such empty return must be on standard form waybills without charges which shall be endorsed "Unassigned Car for Processed Package Food Loading CSD No. 155."

SECTION C. Assigned and Unassigned Cars

- No assessment can be made unless:
 - Waybill or switching document is properly endorsed, and
 - Car is listed in UMLER with proper pool assignment.
- Switching documents covering loaded or empty cars delivered in switching service must be endorsed either "Assigned CSD 155" or "Unassigned CSD 155."
- Car doors must be closed and sealed.
- XF cars are restricted to processed packaged food loading only except, with the approval of owner, such cars may be used for loading other non-contaminating products enclosed in clean packaging. Cars must not be used for the loading of bulk commodities. If cars are used for loading which results in contamination or damage to linings or floors, and it becomes necessary to renew floors, lining or sheathing (including any associated parts), or portions thereof, in order to restore cars to previous loading classification, the cost of repairs shall be assumed by the road (either switching or road haul) placing cars for loading.

EXCEPTION: Carriers may exempt their cars from the provisions of Section A, 2, or B, 2, to allow use by other specified lines for loading as prescribed in Section C. Such exemptions must be confirmed in writing to the Transportation Division in advance.

By direction of,
J. J. Carroll
Executive Director and Chairman

October 12, 1984
File: 580-165

(REVISED)—CAR SERVICE DIRECTIVE NO. 165

(Cancels and Supersedes CSD 165 of November 15, 1980)

TO: Transportation Officers—All Railroads

NOTICE: This Directive is issued under provisions of paragraph (g) Car Hire Rule 19. Each violation of paragraph 2 of the Order Section of this Directive is subject to a graduated scale of assessments, based on violations occurring each calendar month.

EFFECTIVE: 12:01 a.m., November 1, 1984 and continuing until further notice.

APPLICATION: *To cars assigned to shippers or district distribution points carrying railroad reporting marks of the following mechanical designations when pool operator approval is required: FMS, GBR, GBS, GBSR, GDS, GSS, GTS, GWS, HMSR, HTS, PS, RBL, RPS, RPL, RPLX, XL, XL1, XL1H, XP, XPL, XF and all "L" types (except "LO").

ORDER:

- Waybills accompanying assigned cars under load and station copies, must be endorsed "Assigned Car CSD No. 165."
- After unloading, cars subject to this Directive must be:
 - Handled per owner's instructions.
 - Billed back empty to shipper or agent at loading point in reverse of loaded movement.
 - Reloaded by the destination carrier to a point on the railroad or where the destination is closer to the assignment point than the unloading point.
 The holding road to exercise the option.
- When cars are reloaded in accordance with paragraph 2 of this Order, waybills and station copies must be endorsed:
 - Assigned Car CSD No. 165. Reload accomplished. When empty must be loaded to (Agent/Shipper) at (Assignment Point).
- When cars are not reloaded, empty return movement must be on standard form waybills without charges, and must be endorsed as follows:
 - Assigned Car CSD No. 165 returning empty to (Agent/Shipper) at (Assignment Point) via reverse of loaded route.
- After unloading, cars subject to this Directive must be handled in accordance with endorsement instructions on waybill or per owner's instructions. The holding road to exercise the option.
- Car doors must be closed and sealed.
- Switching documents covering loaded or empty car delivered in switching service must be endorsed "Assigned Car CSD No. 165."
- Reclaims for car hire on cars covered by this Directive may be made only in accordance with Car Hire Rule 22 at assignment point, and Car Service Rule 22, as amended, suspended insofar as they conflict with the above provisions.
 - No Car Hire Rule 22 reclaims will be allowed when loaded at other than assignment point.
- XF cars are restricted to processed packaged food loading only except, with the approval of the owner, such cars may be used for loading other non-contaminating products enclosed in clean packaging. Cars must not be used for the loading of bulk commodities. If cars are used for loading which results in contamination or damage to linings or floors, and it becomes necessary to renew floors, lining or sheathing (including any associated parts), or portions thereof, in order to restore cars to previous loading classification, the cost of repairs shall be assumed by the road (either switching or road haul) placing cars for loading.
- No assessment will be made under this Order unless:
 - Waybill or switching document is properly endorsed; and
 - Car is listed in UMLER with proper pool assignment.

EXCEPTION: Owners may exempt certain cars from the provisions of paragraph 2 of this Order to allow use by other specified lines, but such exemption must be confirmed in writing to the Transportation Division in advance.

NOTE 1: Pool operator included to prevent owner from placing his cars under this Directive without pool operator's approval.

*Reissued to add mechanical designation RPC under APPLICATION section.

By direction of,
J. J. Carroll
Executive Director and Chairman

March 2, 1982
File 580-435

CAR SERVICE DIRECTIVE NO. 435

(Cancels and Supersedes CSD NO. 435 of November 30, 1981)

TO: Transportation Officers—All Railroads

NOTICE: This Directive is issued under provisions of paragraph (g) Car Hire Rule 19. Each violation of Section A, 2, or Section B, 2 of this Directive is subject to a graduated scale of assessments based on violations occurring each calendar month.

EFFECTIVE: 12:01 A.M., March 8, 1982 and continuing until further notice.

APPLICATION: To class "LO" and "HTR" covered hopper cars carrying railroad reporting marks.

ORDER:

SECTION A. "Cars Assigned to Shippers, Commodity Pools or Pool Points.

- Waybills accompanying assigned cars under load, and station copies, must be endorsed in one of the following ways:
 - Assigned car CSD No. 435. When empty bill back to (Shipper) at origin of last load in reverse of the loaded movement.
 - Assigned car CSD No. 435. When empty bill back to Agent at (Station) in reverse of the loaded movement.
- After unloading, cars subject to this Directive must be:
 - Handled per owner's instructions.
 - Billed back empty to shipper or agent at loading point in reverse of loaded movement.
 The holding road to exercise the option.
- Such empty return must be on standard form waybills without charges which shall be endorsed "Assigned Car CSD No. 435."

SECTION B. Unassigned Cars.

- Waybills accompanying unassigned cars under load, and station copies, must be endorsed "Unassigned Car CSD No. 435."

- After unloading, cars subject to this Directive must be:
 - Handled per owner's instructions.
 - Returned to home road at any junction.
 - Returned empty to the delivering road at the junction where previous load was received if that junction is not a junction with the home road.
 The holding road to exercise the option.
- Such empty handling must be on standard form waybills without charges which shall be endorsed "Unassigned Car CSD No. 435."

SECTION C. Assigned and Unassigned Cars

- No assessment will be made unless:
 - Waybill or switching document is properly endorsed, and
 - Car is listed in UMLER with proper pool assignment.
- Switching documents covering loaded or empty cars delivered in switching service must be endorsed either "Assigned CSD-435" or "Unassigned CSD-435."
- Agents terminating loads must show on waybills covering all empty covered hopper cars, hopper slides are properly closed. Also, that each covered hopper car is secured to insure against contamination which results when dirt and material pass through these openings.
- To avoid claims due to contamination when covered hopper cars are loaded in one commodity service to another, it is necessary to stress the importance of thoroughly cleaning the cars.

NOTE: Owners may exempt their cars from the provisions of Sections A, 2, and B, 2, to allow use by other specified lines, but such exemptions must be confirmed in writing to the Transportation Division in advance.

*Reissued to add under Section A "Commodity Pools."

By direction of,
J. J. Carroll
Executive Director and Chairman

UMLER SPECIFICATION MANUAL—SECTION V—CONTINUED

ITEM C
Glossary

ASSIGNED CAR

a. **ASSIGNED PRIVATE CAR**—Car (equipped/unequipped) carrying a private reporting mark, leased to a railroad, and assigned to a pool.
b. **ASSIGNED RAILROAD CAR**—Specially equipped car carrying a railroad reporting mark and assigned to a pool under the provisions of Car Service Directives (CSD), (145, 155, 165, or 435).

CAR SERVICE DIRECTIVES—AAR Transportation Division directive's governing the loaded and empty movement of special equipped freight cars.
CAR SERVICE RULE 16—Railroad industry rule governing the assignment of special equipped cars to a shipper.

CAR TYPE CODE—Alpha/numeric code used to designate a specific type of freight car. Designated pool reporter—Pool operator or a railroad designated to administer the reporting of the car assignment for the pool operator.

FORM 16C-10—ADVICE OF POOL ASSIGNMENT NUMBER—Form for manual users reporting submissions to establish, change or delete a pool from UMLER Pool Header File.

FROM POOL—Input seven-digit code identifying pool assignment from which car is being removed. If the car is presently unassigned, input zeroes.

LESSEE—For purposes of this Section, a lessee, is a railroad whose mark is registered in the Lessee field in the UMLER format. A private car with a private mark or without a railroad mark in the lessee field does not qualify for assignment.

MAINTENANCE POOL CODES—One position numeric code used to identify types of maintenance pools as prescribed in Appendix B, Office Manual, AAR Interchange Rules.
MANUAL USER—Railroad that uses form 16C-10 and UMLER Pool Assignment Format 6 as its two input documents for UMLER car pooling transactions.

MECHANICAL DESIGNATIONS—AAR Alphabetic Code which indicates the general category of a car's design.

MESSAGE TRAILER—For UMLER/TRAIN II users, gives the group count in the UMLER 03 and UMLER 04 messages.
ORIGINATING ROAD HAUL CARRIER—Railroad that originates the waybill and becomes the first railroad in the route.

PASSKEY—A four-position alphabetic code assigned to each alphabetic reporting mark. The passkey is confidential to the AAR and the owner of the reporting mark. It is the same passkey used to submit cars to the UMLER file. Passkeys are assigned by the Director TRAIN and UMLER Services, AAR Transportation Division upon written request.

POOL HEADER FILE—The Header File is a master list or file of all existing pools.
POOL LOCATION—The point where pool is actually located and the assigned equipment is held prior to placement at the loading station.

POOL NUMBER (JOINT)—Seven position number assigned by the AAR. Positions 1 through 3 identified as 999 positions 4 through 7 are randomly assigned as pool is assigned.

POOL NUMBER (NATIONAL)—Seven position number assigned by the AAR. Positions 1 through 3 identified as 999 positions 4 through 7 are randomly assigned as pool is assigned.

POOL NUMBER (RAILROAD)—Seven position number assigned by the pool operator for each specific pool. Positions 1 through 3 use the pool operator's AAR Accounting Code Number. Positions 4 through 7 are assigned by the pool operator.

POOL NUMBER (RAILROAD TERMINAL)—Seven position number assigned by the AAR. Position 1 through 4 identified as 9995 positions 5 through 7 are randomly assigned as pool is assigned.

POOL NUMBER (9999001)—Owner has exempted the car(s) from CSD 150, 155 or 435. The applicable Transportation Code is E.

POOL NUMBER (9999002)—Owner has designated the car as an unassigned contaminated car. Applicable to all car types. The applicable Transportation Code is G.

POOL NUMBER (9999003)—Owner has designated that the car(s) FM, FW(S), FO(S) is subject to the provisions of TD No. 3. The applicable Transportation Code is F.

POOL OPERATOR(S)—Four position fields indicating the actual operator of a specific pool. The carrier who holds assigned equipment short of actual placement normally handles reclaim and repair/maintenance pool, may or may not be the carrier actually switching the shippers' plant.

POOL PARTICIPANT(S)—One or more railroads that have agreed to the registration of their cars in a pool.

POOL TYPE CODE(S)—One position alphabetic code used to identify a type of railroad pool assignment.

POOL TYPE (AGENT)—Pool maintained by a railroad with equipment assigned to a location and not a specific shipper.

POOL TYPE (COMMODITY)—A pool of assigned railroad or railroad leased cars maintained for a specific commodity.

POOL TYPE (LOCAL)—Single line pool with no foreign equipment assigned.

POOL TYPE (JOINT)—A pool with more than one pool operator.

POOL TYPE (NATIONAL)—Pool assigned to a specific shipper that does not have a specific loading station.

POOL TRANSPORTATION CODE—One position alphabetic code which describes a specific restriction for a car.

SEASONAL POOL—A pool which is not active year round indicated in the Pool Header Cols. 78-80 as 585 (Report current year).

SHIPPER OR PATRON NAME—The identifier a railroad uses for a particular pool.

TO POOL—Input seven-digit code identifying the pool assignment to which the car is being added. If the car is to be shown as unassigned, input zeroes.

TRAIN II—The railroad industries National Car Information System located at the AAR in Washington, D.C. TRAIN II is Tele Rail Automated Information Network 2nd Phase.

TRAIN MANUAL—AAR Manual describing input and output requirements for the users of TRAIN II.

TRANSPORTATION CODE—Indicates type of assigned service, empty (boxcar) routing or restricted service.

TRANSPORTATION CONDITION CODE—Describes AAR or FRA interchange restriction or distribution code for the handling of equipment.

UMLER/TRAIN II USER—A railroad capable of transmitting via the AAR's TRAIN system.

UMLER—UNIVERSAL MACHINE LANGUAGE EQUIPMENT REGISTER—The railroad industry's car hire, pool assignment, and equipment registration master file.

UMLER 03—UMLER/TRAIN user input message that enables users to submit UMLER pool assignment transactions via the TRAIN network.

UMLER 04—UMLER/TRAIN user input message that enables users to submit UMLER Pool Header File changes via the TRAIN network.

UMLER MESSAGE 74—Valid car assignment or deletion from an established pool. This message is received by a pool operator or designated pool reporter, car owner and/or car lessee and any railroad set-up to receive broadcast 74 messages. Carriers receiving UMLER Message 74 are advised, that the receipt of UMLER Message 74 will duplicate the pool advice.

UMLER MESSAGE 75—Add, delete or change of a pool header by the pool operator or designated pool reporter. This message is received by the pool operator and designated pool reporter and any railroad set-up to receive broadcast 75 messages.

UMLER MESSAGE 76—Pool header error message returned to pool operator or designated pool reporter reflecting errors when attempting to add, delete, or change the actual pool header. This message is received by the pool operator or the designated pool reporter responsible for the pool submissions.

UMLER MESSAGE 77—Car detail error message sent to pool operator, pool reporter, car owner or car lessee when submission of add or delete record for cars or series of cars is in error.

UMLER POOL ASSIGNMENT FORMAT 6—Manual user input document used by a pool operator or designated pool reporter to add or delete a car(s) from a pool.

UNASSIGNED RAILROAD CARS—Special equipped cars not assigned to a pool. The AAR pool digit code would be zeros.

ITEM D
Procedures For Reporting Pool Assignment Numbers

- Form 16C-10 must be furnished to the Transportation Division explicitly providing all detailed information. The AAR will be responsible for maintaining the Pool Participant list. As cars are added to a pool, the owner of the car initial will be added. When deleting ownerships from the pool header each specific reporting mark must be submitted. The deletion of leased private cars must be done on a car-for-car basis. (Carriers reporting via TRAIN see Section 5 of the TRAIN II Manual.)
- The AAR will generate the appropriate transportation codes S or Y cannot be in established by the pool operator. Cars having transportation codes S or Y cannot be in assigned service. Cars qualifying under CSD 145 and 435 in assigned service may have a G transportation code. The G code must be provided by the car owner and can only be assigned to a G type pool. The G code must always be reported when applicable. Transportation codes U (CSD-150 and 155) and W (CSD-435) will be inserted by the AAR when the pool number is zero.
- All inbound AAR forms 16C-10 and UMLER Pool Assignment Format 6 paper documents arriving at the AAR by 12 o'clock noon, E.T., will be processed by close of business with outbound notification being transmitted through daily UMLER messages 74, 75, 76 or 77. If error messages (UMLER message 76 and/or 77) are received, the pool operator, designated pool reporter, car owner or lessee of the cars or series of cars must correct and resubmit UMLER Pool Assignment Format 6 or form 16C-10 for reprocessing by the AAR.

ITEM E

Transportation Code Specifications Applicable to Assignable Cars

- B—Railroad car assigned to a specific shipper at a specific location. Equipment covered under the Provisions of CSD 165. Must be a "B" or "E" type pool.
- C—Railroad car assigned to a specific shipper at a specific location. Equipment covered under the Provisions of CSD 145 and 435. Must be a "C" or "L" type pool.
- D—Private car, leased to a railroad, assigned to a specific shipper at a specific location. Must be a "C" or "L" type pool.
- E—Cars exempt from provisions of CSD 150, 155 or 435 and flat cars subject (FC-FCA) to provisions of SCO-110.
- G—Cars used for loading of contaminated commodities as provided in Car Service Rule 14. Pool Code number may be blank. If pool Code number is not blank, pool must be a G type pool.
- H—Cars restricted to a Commodity Pool. Must be "H" type pool. Cars are covered under the Provisions of CSD 155.
- J—Car restricted to an Agent's Pool. Must be "J" type pool. Cars are covered under the Provisions of CSD 165.
- N—National Pool car assigned to a specific shipper but not to a specific point. Must be a "N" type pool. If railroad car, must be covered under the Provisions of CSD 145 or 435.
- P—Car restricted to a Commodity Pool. Must be a "P" type pool. If railroad cars, must be covered under the Provisions of CSD 145, 155 or 435.
- R—Car restricted to an Agent's Pool. Must be "R" type pool. If railroad cars, must be covered under the Provisions of CSD 145, 155 or 435.
- U—Unassigned railroad car to be handled in accordance with the applicable Provisions of CSD 150, 155.
- W—Unassigned railroad car to be handled in accordance with the applicable Provisions of CSD 435.
- X—Cars assignable to all pool types, except N—National Pool.

ITEM F

Mechanical Designations and Corresponding AAR Type Codes For Equipment Carrying Railroad Reporting Marks Covered Under Provisions of CSD 145, 150, 155, 165 and 435.

CSD 145, 150, 165		E 2		All "L" EXCEPT "LO"	
XL = A 3	GBR = E 3	HMSR = K 7	LF = L 0		
XL = A 4	GBS = E 4	HTS = K 4	LG = L 1		
XP = A 0	GDS = E 5	FA = V	LP = L 2		
XPI = A 1	GSS = E 6	FB = F 4	LPS = L 3		
RB = R 0	GTS = E 7	FBS = F 5	LU = L 4		
RPL = R 1	GW = E 8	FC = P	LM = L 5		
RPL = R 7		FCA = O	LC = L 6		
RPC = R 8		FL = F 7	LS = L 9		
		FMS = F 2			

CSD 155, 165

XF = A 2

CSD 435

LO = C 1

HTR = C 2

ITEM G

Pool Type Codes

Pool Type Code	Pool Description	Applicable UMLER Transportation Codes
C	A Pool comprised of equipment assigned to a specific shipper at a specific location. If railroad car, must be covered under Provisions of CSD 145 or 435.	XA, C, D
B	A Pool comprised of equipment assigned to a specific shipper at a specific location. If a railroad car, must be covered under the Provisions of CSD 165.	B
G	A Pool where all types of contaminated cars are loaded.	G
L	Similar to the C Pool, except no foreign cars assigned to the operating carrier may apply. Restriction Code "C" is applicable because assigned car pool may change their status from "L" to "C" and vice versa.	XA, C, D
E	Similar to the B Pool, except no foreign cars assigned to the operating carrier may apply. Restriction Code "B" is applicable because assigned car pools may change their status from "E" to "B" and vice versa. If railroad car, must be covered under the Provisions of CSD 165.	B
N	Similar to the "C" and "L" Pools, except the cars are not assigned to a specific loading point (National Pools).	N
T	A Pool comprised of equipment assigned to an agent. If railroad car, must be covered under the Provisions of CSD 145, 155 or 435.	R
J	A Pool comprised of equipment assigned to an Agent. If railroad car, must be covered under the Provisions of CSD 165.	J
P	A Pool comprised of equipment assigned to accommodate a specific commodity. If railroad car, must be covered under the Provisions of CSD 145, 155 or 435.	P
H	A Pool comprised of equipment assigned to accommodate a specific commodity. If railroad car, must be covered under the Provisions of CSD 165.	H

UMLER SPECIFICATION MANUAL—SECTION V—CONTINUED

ITEM H

Manual Users

1. Users Reporting Requirements for Creating, Changing, or Deleting a Pool (Pool Header Master). Only the pool operator may establish, change or delete a pool. The pool operators Passkey as assigned by the Director TRAIN and UMLER Services must be reported with each transaction.

A. Establishing a New Pool. All pool operators should review Items D through G before completing form AAR-16C-10. The pool operator must complete AAR form 16C-10—Advice of Pool Assignment Number—following the descriptive keypunch format described therein. The AAR will process a pool operator's completed form 16C-10 and provide the pool operator with the applicable UMLER message 75 (valid transaction) or message 76 (error transaction). (See Item C-Glossary).

B. Changing an Established Pool. The pool operator must submit form 16C-10, which is used for changing the following: shipper or patron name, state, loading station, pool operator(s) pool type, pool location, maintenance pool codes and/or designated pool operator(s). The pool number cannot be changed unless a delete is being effected. The AAR will process pool operator's submitted 16C-10, and provide pool operators with the applicable UMLER message 75 (valid transaction) or 76 (error transaction).

C. Deleting a Pool. The pool operator must complete AAR form 16C-10, which is used to remove an established pool from the Pool Header File. The AAR will process form 16C-10, and the pool operator will be informed of the transaction via the applicable UMLER message 75 (valid transaction) or 76 (error transaction).

2. Adding and Deleting a Car(s) or Series of Cars from an Established Pool

A. Adding a Car(s) or Car Series to an Established Pool. The Pool operator or designated pool reporter are the only railroads that can add a car(s) or series of cars to an established pool. The pool operator or designated pool reporter must complete the AAR Pool Assignment Form 6 form to add equipment. They must have a valid Passkey and pool number. Pool Assignment Form 6 must be completed following the descriptive keypunch format outlined therein. The pool operator, designated pool reporter, and car owner and/or lessee of the car, will receive notification of transactions via AAR UMLER message 74 (valid transaction) or 77 (error transaction).

B. FROM and TO Pool Assignment for a Car(s) or Series of Cars. The pool operator or designated pool reporter must complete the UMLER Pool Assignment Form 6 when changing a car's pool assignment. The AAR will inform the pool operator, designated pool reporter and car owner and/or lessee of the car through the applicable UMLER message 74 (valid transaction) or 77 (error transaction) concerning the status of its transaction to UMLER.

C. Requirements for Deleting a Car(s) or Series of Cars from an Established Pool. The following are permitted to delete a car(s) or series of cars from an established pool: pool operator, designated pool reporter, car owner, or lessee of a car.

a. Pool Operator or Designated Pool Reporter. Must complete UMLER Pool Assignment Form 6 for deletion of a car(s) or series of cars from its pool. (Note: If an established pool is deleted from the Pool Header by the pool operator all cars or series of cars either system or belonging to pool participants are deleted from that pool. Individual pool participants wishing to delete a car(s) or series of cars must complete UMLER Pool Assignment Form 6).

b. Car Owner or Lessee of a Car. Must complete UMLER Pool Assignment Form 6 to delete a car(s) or series of cars from a pool.

3. Requirements for Reporting Joint, National and Railroad Terminal Pools.

A. Joint Pool (998 Pool). A joint pool number can only be assigned by the AAR after the receipt of the written or wire concurrence of each carrier participating as a pool operator. A joint pool is identified by a unique pool number "998" in position 1-3 of the pool number, with positions 4-7 randomly assigned by the AAR. Cars being reported to a "998" pool are reported by the carrier designated as the pool reporter. The car owner has the option to remove its car(s) from such pools.

B. National Pool (999 Pool). A National Pool number can only be assigned by the AAR after the receipt of written or wire request from a carrier. The National Pool is identified by a unique pool number "999" in position 1-3 of the number, with position 4-7 randomly assigned by the AAR. Cars being reported to a "999" pool must be reported by the car owner. Privately-owned cars having railroad reporting marks in the lessee field must be reported by the railroad indicated in the lessee field.

C. Railroad Terminal Pool (995 Pool). A Railroad Terminal Pool number can only be assigned by the AAR after receipt of written or wire request from carrier. The carrier requesting the establishment of this type pool will be the designated pool reporter. A Railroad Terminal Pool is identified by a unique pool number "995" in positions 1-4 of the pool number and positions 5-7 randomly assigned by the AAR. The car owner has the option to remove its car(s) from such pools.

4. Requirements for Reporting Exempted 150, 155 & 435 Cars, Unassigned Contaminated Cars & TD-3 Flat cars through the Pool System.

A. Pool 999001 report this pool number to exempt car types subject to CSD 150, 155 and 435, generates the transportation code E and no pool number.

B. Pool 999002 report this pool number to designate an unassigned car to a contaminated status, generates the transportation code G and no pool number.

C. Pool 999003 report this pool number to place the flat car (FM, FDIS), FW(S) under the provisions of TD-3, generates a transportation code F and no pool number.

NOTE: The transportation codes E, F, G can be reported via UMLER transactions as opposed to the pool system.

ITEM I.

Automated UMLER/TRAIN II Users

1. Automated Users Reporting Requirements for Creating, Changing or Deleting a Pool (Pool Header Master). The TRAIN Manual Section V, Message Types, III, Pool Header File Changes (UMLER Transactions). A. Original Entry (To AAR). UMLER O4 message format must be used as input to the TRAIN system to create, change or delete a pool. Only the pool operator can make such transactions.

A. The pool operator will receive UMLER format 75 (valid transaction) and 76 (error transaction) messages from the AAR confirming such transactions (See Part C, Glossary). TRAIN Manual gives a detailed format of 75 and 76 messages as AAR output.

B. Automated users who receive UMLER 76 messages should cause user to correct UMLER O4 message and resubmit.

2. Automated Users Reporting Requirements for Adding or Deleting a Car(s) or Series of Cars from an Established Pool. The TRAIN Manual, Section V, Message Types, II, Pool Assignment (UMLER Transactions). A. Original Entry (To AAR). UMLER O3 message format must be used as input to the TRAIN system to add or delete a car(s) or series of cars from an established pool.

A. Adding a Car(s) or Series of Cars. Only the pool operator or designated pool operator can add a car(s) or series of cars to a pool, and must use UMLER O3 message as input to TRAIN.

B. Deleting a Car(s) or Series of Cars. The pool operator, designated pool operator, car owner, or lessee of a car(s) or series of cars can delete such cars from an established pool. UMLER O3 message is used as input to TRAIN.

C. Automated users will receive applicable UMLER format 74, 75, 76, or 77 messages as output from TRAIN II and all 76 or 77 error messages received should cause user to correct UMLER O3 transaction and resubmit. TRAIN Manual gives a detailed format of 74, 75, 76 and 77 messages as AAR output.

D. All UMLER O4 and O3 messages must be received as input by 8:00 P.M., E.T. in order to be processed with that days activity.

NOTE 1: For requirements for reporting Joint, National and Railroad Terminal Pools see ITEM H. 3.

NOTE 2: For requirements for reporting exempted CSD-150, 155 and 435 cars, unassigned contaminated cars and TD-3 flat cars see ITEM H4.

E. UMLER ERROR CODES. (See Table V-D, Train II Users Guide).

ERROR

EXPLANATION

- 01 The "from" or "to" pool number is not numeric or is not in the AAR Pool Header Master record is rejected.
- 02 For other than national pools, the resubmitting road is not the pool operator's owner/lessee-record is rejected.
- 03 Shipper name was blank.
- 04 Car is advanced reported. Warning only.
- 05 Car is not eligible for assignment according to Car Service rules.
- 07 The submitting road is not the car owner or the operator of the "from" pool.
- 08 Not used.
- 09 The "to" pool was a national pool, but the submitting road was not the owner.
- 10 Operator #4 was invalid.
- 11 C-designator was not blank of 'C' or 'B'.
- 12 G-designator was not blank or 'G'.
- 13 L-designator was not blank or 'L' or 'E'.
- 14 N-designator was not blank or 'N'.
- 15 T-designator was not blank of 'T' or 'J'.
- 16 P-designator was not blank of 'P' or 'H'.
- 17 Either no valid designator, or more than one valid designator.
- 18 Maintenance type code invalid.
- 19 Pool number does not begin with submitter's Rule 260 number.
- 20 State was blank or invalid.
- 21 Loading station was blank.
- 22 Operator #1 was blank or not on AAR table of valid operators.
- 23 Operator #2 was not on AAR table of valid operators.
- 24 Operator #3 was not on AAR table of valid operators.
- 25 For an add, the participant is already in the list.
- 26 For a delete, the participant was not in the list.
- 27 For an add, the maximum number of participants is 50. Add rejected.
- 31 The CAR HIGH NUMBER was not greater than or equal to CAR LOW NUMBER. Detail record was rejected.
- 32 The car series contained more than the maximum number allowed. Detail record was rejected.
- 33 Break the series into several smaller ones, each of which contains 1000 or less cars.
- 47 A change Detail record was submitted for a car which is not on the Detail record. Detail record was rejected.
- 58 Car already assigned to transaction "to" pool. This is an UMLER error. Necessary activity. No updating has occurred.
- 60 Record was rejected because car may not be placed in pool assignment transaction Codes E, M or O.
- 67 Car has a Transportation Code of G and may not be assigned to a pool.

F. Owners reporting O3 Pool messages that update their UMLER Pool Header Master messages can report "ZZ" in the Transportation Code and Transportation Code fields. This will ensure that the car assignment will not be changed.

ITEM J

AAR Output Files From The Pool System

1. Pool Header File. This file indicates the Pool Number, Shipper, State, Operating Roads, Pool Type, Pool Location and list of Participants.
 - A) Tape. The Pool Header Master Tape is available on a monthly basis, effective as of the first of each month.
 - B) Micro-Fiche. The Pool Header Master Micro-Fiche is available on a monthly basis, indicating a complete File effective as of the first of each month.
2. 16C Car Assignment Master. This File indicates all assigned cars in appropriate pool number, is sequenced by reporting mark with appropriate pool number.
 - A) Tape. The 16C Car Assignment Master Tape is available on a monthly basis, effective on the first of each month.
 - B) Micro-Fiche. The 16C Car Assignment Master Micro-Fiche is available on a monthly basis, indicating a complete File as of the first of each month.
3. JEUP 3000. This File indicates sequenced pool numbers with the assigned cars assigned to each pool number. This File is only available in Micro-Fiche form, and is available on a monthly basis, indicating a complete File effective as of the first of each month.
4. 16C Pool Activity History. This listing indicates all changes to car assignment on a monthly basis. This list is only available in Micro-Fiche form, and is available on a monthly basis, effective at the end of each month.

Note: For subscription or request information contact Director of TRAIN and UMLER Services, Transportation Division, Association of American Railroads, 500 F Street, N.W., Washington, D.C. 20001.

ITEM K

16C-10 Format And Input Instructions
(Revised Effective January 1, 1982)

Passkey: A four-character alphabetic code assigned to each alphabetic reporting mark. The passkey is confidential to the AAR and the owner of the reporting mark. It is the responsibility of the owner of the reporting mark to submit cars to UMLER file. Passkeys are assigned by the Director TRAIN and UMLER Services Transportation Division, upon written request.

Position	Description
1-7	Pool Number—Positions 1 through 3 are the pool operators AAR Accounting Code Number, positions 4 through 7 are pool numbers assigned by the pool operator.
8-27	Shipper or Patron Name—This field identifies the company, railroad agent, commodity pool.
28-29	State—This field is used to indicate state or province where the pool is located.
30-44	Loading Station—The actual shipper loading point or plant location or holding point.
45-60	Pool Operators—Four position fields indicating the actual operator of the pool. The carrier who holds assigned equipment short of actual placement handles reclaim and repair/maintenance pool, may or may not be the carrier switching the shippers' plant.
61-66	Pool Type Code—One position field used to identify pool type.
*67-80	Pool Location—The point where the pool is actually located and the assignment is held prior to placement at the loading station.
81	Transaction Code—One position field indicating: Delete-1, Add-2, or Change-3.
82	Maintenance Pool Codes are as follows: <ol style="list-style-type: none"> 1—Automobile and truck loading multi-level flat cars 2—Specialty equipped chain tie down cars (farm implement type) 3—Cars equipped with cross bar interior restraining devices 4—Automobile parts and bulkhead equipped cars 5—Automobile and truck frame cars 6—Flat cars equipped for saddleback loading
83	Blank
84-193	Pool Participant: The standard alphabetic reporting mark for cars that are added or deleted to the pool, e.g.: <ol style="list-style-type: none"> 2 A T S F 2 B O 1 M P
194-197	Designated Pool Reporter: Assigned alphabetic reporting mark of the carrier reporting for non-mechanized operators that have made such an agreement with the reporting road.

UMLER SPECIFICATION MANUAL—SECTION V—CONTINUED

If the pool is a seasonal pool i.e., no cars are assigned at various intervals, report S and the current year, e.g., S84 in Cols. 78-80.

Note 1: To add or change a pool header the following data must be provided: Passkey and information in positions 1-82 and 194-197, except, the pool location (67-80), maintenance code (82) and/or designated pool reporter (194-197) can be left blank if not applicable.

Note 2: To delete a pool header the complete record can be provided or, a minimum of information as follows: Passkey, Pool Number and Delete Transaction Code 1.

Note 3: Pools identified as CSD 145 and/or CSD 435 type pools (C, G, L, N, P, and T) cannot be changed to CSD 165 type pools (B, E, J, and H) without the concurrence of all pool participants.

Note 4: To remove a pool participant from the header file report the following: Passkey, Pool Number (1-7), Transaction Code "1" from one of the applicable transaction fields in pool participant section of Form 16C-10. The delete transaction will not effect the remaining fields of the header. However, this transaction will generate a transaction to remove the cars assignment for each reporting mark deleted from the pool header. This method cannot be used to delete private cars from assignment.

Date: _____
Request No. _____

ITEM K

Revised Effective: January 1, 1982

FORM 16C-10

ADVICE OF POOL ASSIGNMENT NUMBER

This Information Can Only Be Submitted By The Pool Operator -- PASSKEY=

POOL NUMBER										SHIPPER OR PATRON NAME																			
LOCATION STATION										POOL OPERATOR(S)																			
OPERATOR(S)										POOL TYPE										POOL LOCATION									
										C B G E N T J P H																			
										61 62 63 64 65 66										67 78 80									

If the pool is a seasonal pool i.e., no cars are assigned at various intervals, report S and the current year, e.g., S86 in Cols. 78-80.

(Circle One)

TRANSACTION CODES: 1= Delete; 2= Add; 3 = Change -- This field must be completed.

Maintenance Pool Code, Appendix B, Office Manual, AAR Interchange Rules -- Indicate the appropriate Rule Number as the code (Rule 1 through 6)

Blank

DELETING POOL PARTICIPANTS

When deleting pool participants each reporting mark must be indicated with a code one (1), e.g.,

1 1 A T S F 1 1 I C 1 1 I C G 1 1 G M O 1 1 1 1 1 1									
84 89 94 99 104									
89 94 99 104 109									
119 124 129 134 139									
149 154 159 164 169									
179 184 189									
Designated /Pool Reporter /Operator 194									

SUBMITTED BY:

Name: _____, Title: _____

Company: _____, Telephone: () - _____

UMLER SPECIFICATION MANUAL—SECTION V—CONTINUED

ITEM L

Revised Effective January 1, 1982

UMLER Pool Assignment Format 6 Input Instructions (Revised)

Position	Description
1	Transaction type code must equal: D
2	Transaction activity code must equal: 3
3-6	Alphabetic reporting mark or the stencilled reporting mark on the car being assigned.
7-12	A six position numeric field with leading zeroes of the car or first number of a consecutive car series being added or removed from the pool.
13-18	A six position numeric field with leading zeroes of the ending consecutive car number series being reported.
19-25	From Pool Number—1) Input seven-digit code identifying pool assignment from which car is being removed. If the car is presently unassigned, input zeroes.

26-32	To Pool Number—Input seven-digit code identifying the pool assignment to which the car is being added. If the car is to be shown as unassigned, input zeroes.
33-36	Alphabetic reporting mark of the pool operator or designated pool operator reporting the car assignments to the UMLER file.
37-40	Passkey is the confidential code assigned to pool operator or designated pool operator.
41-80	Blank for document.
41-328	Complete UMLER tape length space fill.

UMLER POOL ASSIGNMENT FORMAT 6 (Revised)

Document and Tape Formats

Revised October 1, 1980

Passkey=_____

T T T C		CAR			POOL NUMBER		POOL	OPERATOR'S	
R Y R O	A P A D	INITIAL	LOW NUMBER	HIGH NUMBER	FROM	TO	INITIAL	PASSKEY	
N E N E	S S								
1	2	3-6	7-12	13-18	19-25	26-32	33-36	37-40	
1	D 3								
2	D 3								
3	D 3								
4	D 3								
5	D 3								
6	D 3								
7	D 3								
8	D 3								
9	D 3								
10	D 3								
11	D 3								
12	D 3								
13	D 3								
14	D 3								
15	D 3								
16	D 3								
17	D 3								
18	D 3								
19	D 3								
20	D 3								

UMLER SPECIFICATION MANUAL—SECTION V—CONTINUED, SECTION VI—EXHIBITS

ITEM M
TRANSPORTATION CODE T

The T Transportation Code applies to plain Box cars (CTC B____) and Specially Equipped Boxes (CTC A____, F_1____, F_2____, F_4____, F_5____, F_7____, L_4____, L_7____, R_0____, R_1____, R_5____, R_7____, R_8____) when exempt from CSD 150, 155, shown by a Transportation Code 'E' and unequipped flat cars (F_0____) without the Transportation Code 'F'. When an empty car is not needed for reloading, and TRAIN II finds the owner involved in the previous loaded movement, the car is returned in exact reverse route and junction of that movement.

If TRAIN II does NOT find the car owner in the previous loaded movement, a message is sent to UMLER which inserts a Transportation Code 'T'. Empty cars not needed for reloading that carry a T code are returned via SCO-90/Rule 2.

The 'T' code will be overridden by codes S, X, Y, and M. Records having an E or O code will get the T and E or O will be moved to the Transportation Condition Code field on the output record until the 'T' code is removed by the AAR.

Revised Edition of Special Car Order No. 90

Effective September 1, 1986

Previous Issues of This Order Are Cancelled

This Issue Contains All Changes

TO: All Railroads

Car Service Rule 2 is hereby suspended with respect to all: XM-XMI box cars; mechanical designations FB, FBS, FL, FMS, LC, LU, RB, RBL, RPB, RPC, RPL, XF, XL, XLJ, XP and XPI cars when exempt from CSD 150 or 155 ("E" Transportation Code in the UMLER file); and FM flat cars not carrying Transportation Code "F." The following are the instructions for handling the above foreign cars not needed for loading:

1. When the owning road participated in the last loaded movement, forward the car to the road from which originally received, at the junction where received, until delivered to the owner (includes cars terminated in switch service at a junction with the owner).
2. When the owning road did not participate in the last loaded movement:

(A) Carriers that are a direct connection of the owner shall:

- (1) Forward car to the home road at any junction, except cars handled in switch service must be returned to the delivering road.
- (2) Forward car to the road from which originally received under load, at the junction where received, except that when handled in road haul service, cars of direct connection ownership may not be delivered empty to a road which does not have a direct connection with the car owner. If the junction where received under load is also a junction with the car owner, car must be delivered to the owner at that junction.

(B) Carriers that are an indirect connection of the owner:

- (1) A carrier provided outlets for an ownership listed in Special Car Order 90 shall use the designated outlet, except cars received loaded for release in switch service by a carrier which has not been provided an outlet under SCO 90 at that terminal may return such cars to the delivering road haul carrier. If the carrier handling the car in switch service has an outlet under SCO 90 at that terminal it shall be used.
- (2) A carrier not provided outlets in Special Car Order 90 shall forward the car to the road from which originally received under load, at the junction where received.

NOTE 1: Applicable box cars exceeding Plate C. Reverse routing of car(s) is permissible, when the dimensions of such car(s) prohibit clearance by specified SCO 90 route.

In order to facilitate the identification of cars moving under paragraph 2, cars in which the owner was not involved in the last loaded movement will carry a Transportation Code of "T" in UMLER file. The Committee on Car Service has stated that the UMLER file will govern regarding the proper handling of equipment in interchange service.

By direction of,
J. J. Carroll
Executive Director and Chairman

EXHIBIT A
Tape Formats

INPUT (to AAR Data Center): Input tape submissions to the AAR for inclusion in the UMLER system must meet the following requirements to ensure correct processing by the AAR:

- A. The external label on the input reel must contain: 1) the name of the submitting company, 2) the data set name of the submission, 3) the volume serial number of the input reel, 4) the track and density of the input reel, 5) the creation date of the input reel. No printouts should be furnished. Copy of covering letter must be included. (See Exhibit K).
- B. Input reels for UMLER submissions must be 9 track, 1600 or 6250 bits per inch with IBM OS Standard Labels. The files which will be accepted as input are as follows:

1. UMLER tape format for trailers, containers, and all rail cars:

Data Set Name is 'UMLER, Trans. F328'

First record must be the PASSKEY

Record Length is 328 characters

Blocking Factor is 10 records per block

No record separators

Padding with 9's is optional

In sequence by CAR or UNIT INITIAL (major) and LOW NUMBER (minor)

See the UMLER TAPE INPUT formats A, B, C, and D

2. UMLER line-image tape format for all trailers, containers and rail cars:

Data Set Name is 'UMLER, TRANS. F80'

First record must be the PASSKEY

Record Length is 80 characters

Blocking Factor is 20 records per block

No record separators

Padding with 9's is optional

In sequence by CAR INITIAL (major), LOW NUMBER (intermediate) and LINE NUMBER (minor)

See the UMLER LINE INPUT formats A, B, C, and D

OUTPUT (from AAR Data Center): Output tapes received by UMLER users from the AAR will conform to the following conventions:

- A. The external label on the output reel will contain: 1) the name of the receiving company, 2) the data set name of the file contained on the output reel, 3) the volume serial number of the output reel, 4) the track and density of the output reel, 5) the creation date of the output reel.
- B. Output files from UMLER will be produced on 9 track, 1600 and 6250 bits per inch, IBM OS Standard Labeled tapes only. The files that will be produced as output are:

1. UMLER master file tape (contains all registered equipment for all roads and companies):

Data Set Name is 'AAR, UMLER, MASTER'

Record Length is 235 characters

Blocking Factor is 10 records per block

No record separators

No padding if last block is short

See the UMLER TAPE OUTPUT formats A, B, C, and D

In sequence by CAR or UNIT INITIAL (major) and LOW NUMBER (minor)

Tape characters will be represented as EBCDIC

2. UMLER transactions applied successfully to the UMLER master file:

Data Set Name is 'AAR, CHANGES, U____' where U____ is replaced by the Update Number, e.g., U221

Record Length is 235 characters

Blocking Factor is 10 records per block

No record separators

No padding if last block is short

See the UMLER TAPE OUTPUT formats A, B, C, and D

In sequence by CAR or UNIT INITIAL (major) and LOW NUMBER (minor)

Tape characters will be represented as EBCDIC

3. UMLER fleet copy (all records in the UMLER master pertaining to a requesting road or company):

Data Set Name is 'UMLER, FLEET, ALCAR'

Record Length is 400 characters

Blocking Factor is 10 records per block

No record separators

No padding if last block is short

See the UMLER TAPE INPUT formats A, B, C, and D

In sequence by CAR or UNIT INITIAL (major) and LOW NUMBER (minor)

Tape characters will be represented as EBCDIC

NOTE: Tape contains AAR generated fields as follows:

Positions	Description
329	Special Indicator Code 1 for private tank cars having a value less than \$18,001.
329-332	Hourly Charge
333-336	Mileage Charge
337-340	Appurtenance Charge
341-344	Mechanical Designation
345-351	Pool Code Number
352	Ownership Code
353-354	Rate Formula Code
355-358	OT-37 Surcharge
359	Blank

UMLER SPECIFICATION MANUAL—EXHIBIT A—CONTINUED

UMLER FORMAT A
CARD, DOCUMENT AND TAPE INPUT FORMAT
FOR ALL CARS EXCEPT TANK AND FLA1 CARS

[illegible]

UMLR SPECIFICATION MANUAL—EXHIBIT A—CONTINUED

[illegible]

UMLER SPECIFICATION MANUAL—EXHIBIT A—CONTINUED

UMLER FORMAT C
CARD, DOCUMENT AND TAPE INPUT FORMATS
FOR FLAT CARS

PASSKEY	CAR		
	INITIALS	LOW NUMBER	HIGH NUMBER
1			
2			
3			
4			
5			
6			
7			
8			
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EFFECTIVE: 10/01/84

[illegible][illegible][illegible][illegible][illegible]

UMLER SPECIFICATION MANUAL—EXHIBIT A—CONTINUED

UMLER FORMAT D
CARD, DOCUMENT AND TAPE INPUT FORMATS
FOR TRAILERS/CONTAINERS

PASSKEY	T R A N S P O R T A T I O N	CAR			
		INITIALS	LOW NUMBER	HIGH NUMBER	
2	13	7		13	

EFFECTIVE: 10/01/86

--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--

UMLR SPECIFICATION MANUAL—EXHIBIT A—CONTINUED

UNLFR FORMAT E
DOCUMENT AND TAPE INPUT FORMAT
LOCOMOTIVE

[illegible]

EFFECTIVE: 10/01/86

[illegible]

1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85															

[illegible][illegible]

1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87													

UNLIR FORMAT F
DOCUMENT AND TAPE INPUT FORMAT
FOR ARTICULATED INTERMODAL CARS

EFFECTIVE: 10/01/86[illegible][illegible][illegible][illegible]

EFFECTIVE: 10/01/86

[illegible]

UNIVERSAL MACHINE LANGUAGE EQUIPMENT REGISTER

UMLER FORMAT D -- -- REVISED
TAPE OUTPUT -- -- AAR UMLER MASTER & CHANGES
HIGHWAY TRAILERS AND CONTAINERS

EFFECTIVE: 10/01/86

[illegible][illegible][illegible]

UMLER SPECIFICATION MANUAL—EXHIBIT A—CONTINUED

UMLER FORMAT E
TAPE OUTPUT FORMAT
AAR MASTER AND CHANGES
LOCOMOTIVES

EFFECTIVE: 10/01/86

UNIT	OWNER	LESSEE	CAR TYPE	LOW NUMBER	HIGH NUMBER	PER DIEM	MILEAGE	HORSE-POWER	RAIL	LENGTH	WIDTH
1213	17	11	15	1920	26	323335	3738	414345	495153	565860	
A	A	A	A	A	N	N	N	N	N	N	N

[illegible][illegible]

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UMLER SPECIFICATION MANUAL—EXHIBIT A—CONTINUED

ARTICULATED INTERMODAL CARS

EFFECTIVE: 10/01/86

[illegible]

UMLER SPECIFICATION MANUAL—EXHIBIT C

Revised October 1, 1985
Effective December 1, 1980

Passkey= - - - -

UMLER DELETION INPUT FORMAT

LINE	T r a n s	Car Initial	Low Number	High Number	CTC or Diff	LINE	T r a n s	Car Initial	Low Number	High Number	CTC or Diff
(1)	(2)	(3-6)	(7-12)	(13-18)	(19-22)	(1)	(2)	(3-6)	(7-12)	(13-18)	(19-22)
1	1					1					
2	1					1					
3	1					1					
4	1					1					
5	1					1					
6	1					1					
7	1					1					
8	1					1					
9	1					1					
10	1					1					
11	1					1					
12	1					1					
13	1					1					
14	1					1					
15	1					1					
16	1					1					
17	1					1					
18	1					1					
19	1					1					
20	1					1					

- NOTE: 1) To delete a single car, report the car type code or 0000 in Cols. 19-22.
 2) To delete a car series, report the difference in Cols. 19-22, e.g. ABC 10-15=0005
 3) Line Col. 1, must be 1.

UMLER DELETE TRANSACTION CODES

Transaction Code	Reason for Deletion
A	Miscellaneous deletions errors in listing, release of reserved number, renumbered
B	Obsolete but serviceable, held in storage low capacity, small doors
C	Car deleted for use in company service
D	Destroyed, wrecked
L	Lease terminated, removed from fleet
P	Retired unserviceable beyond economical repair limits
R	Rebuilt
S	Sold serviceable
T	Rehabilitated under Circular OT-37
W	Over age retired for dismantling
1	Reason unknown or owner does not desire to furnish data

The appropriate code will be entered in the transaction code field of the present UMLER record by the car owner and made a part of the car history file. Subsequently, analysis of the history file will be made, for example of B, P, R and W records, to develop car life data.

NOTE: The alphabetic transaction codes do not apply to TOFC/COFC equipment.

ASSOCIATION OF AMERICAN RAILROADS CAR TYPE & TRAILER & CONTAINER TYPE CODES

EQUIPPED BOX CARS CAR TYPE CODE A _ _ _ _

First Numeric

- 1—Less than 49' inside length
- 2—Less than 49' inside length with cushion draft gear/underframe
- 3—49' and less than 59' inside length
- 4—49' and less than 59' inside length with cushion draft gear/underframe
- 5—59' and less than 79' inside length
- 6—59' and less than 79' inside length with cushion draft gear/underframe
- 7—79' and over, inside length
- 8—79' and over, inside length with cushion draft gear/underframe
- 9 & 0—Not Used

Second Numeric

- 0=XP
- 1=XPI
- 2=XF
- 3=XL
- 4=XLI
- 5 thru 9=Not Used

Third Numeric

- 0—Other type door/opening
- 1—Sliding door, opening, less than 9'
- 2—Sliding door, opening, 9' less than 11'
- 3—Sliding door, opening, 11' and over
- 4—Plug door, opening, less than 9'
- 5—Plug door, opening, 9' less than 11'
- 6—Plug door, opening, 11' and over
- 7—Combination (Sliding-Plug) doors
- 8 & 9—Not Used

UNEQUIPPED BOX CARS CAR TYPE CODE B _ _ _ _

First Numeric

- 1—Less than 49' inside length
- 2—Less than 49' inside length, cushion draft gear/underframe
- 3—49' and less than 59' inside length
- 4—49' and less than 59' inside length, cushion draft gear/underframe
- 5—59' and less than 79' inside length
- 6—59' and less than 79' inside length, cushion draft gear/underframe
- 7—79' and over, inside length
- 8—79' and over, inside length, cushion draft gear/underframe
- 9 & 0—Not Used

Second Numeric

- 0—XM—Sliding doors, inside width less than 9'06"
- 1—XM—Sliding door, inside width 9'06" & over
- 2—XM—Plug doors, inside width, less than 9'06"
- 3—XM—Plug doors, inside width 9'06" & over
- 4—XM—Combination (sliding-plug) doors, inside width less than 9'06"
- 5—XM—Combination (sliding-plug) doors, inside width 9'06" & over
- 6—XM—Other door, any width
- 7—XMI—inside width less than 9'06"
- 8—XMI—inside width 9'06" and over
- 9—Not Used

Third Numeric

- 0—Other type door/opening
- 1—Doors less than 8' opening
- 2—Doors 8' less than 9' opening
- 3—Doors 9' less than 10' opening
- 4—Doors 10' less than 11' opening
- 5—Doors 11' less than 13' opening
- 6—Doors 13' less than 15' opening
- 7—Doors 15' & over opening
- 8 & 9—Not Used

COVERED HOPPER CARS CAR TYPE CODE C _ _ _ _

First Numeric

- 1=Gravity Unloading—non-pressurized gravity unloading.
- 2=Pneumatic Unloading—non-pressurized, for unloading by means of vacuum or suction equipment with receiver's facilities without capability of gravity discharge into a hopper.
- 3=Gravity-Pneumatic Unloading—non-pressurized car with capabilities either for unloading by means of vacuum or suction in conjunction with receiver's facilities or operation as a straight gravity mode.
- 4=Fluidized-Gravity Unloading—Air fluidization to expedite unloading; non-pressurized except in fluidization chambers, with gravity outlet.
- 5=Fluidized=Pneumatic Unloading—Air fluidization to expedite unloading; non-pressurized except in fluidization chambers, with means for unloading by means of vacuum or suction in conjunction with receiver's facilities.
- 6=Pressure Differential—Car body pressurized to 5 psi. or greater, with or without supplementary fluidization; discharge through pneumatic pipes.
- 7=Other Unloading Systems—Any discharge system not defined by 1 through 6 above.
- 8, 9 & 0=Not Used

Second Numeric

- 1=LO (Covered Hopper)
- 2=HTR (Equipped with non-permanent roof for LO service)
- 0, 3 thru 9=Not Used

Third Numeric

- 1=Less than 3,000 cu. ft. capacity
- 2=3,000 but less than 4,000 cu. ft. capacity
- 3=4,000 but less than 5,000 cu. ft. capacity
- 4=5,000 cu. ft. capacity and over
- 0, 5 thru 9=Not Used

D—LOCOMOTIVE

First Numeric

- 0=Not Used
- 1=Freight Locomotive
- 2=Passenger Locomotive
- 3=Switching Locomotive
- 4=Non-Cab Freight Locomotive
- 5=Non-Cab Passenger Locomotive
- 6=Auxiliary Unit (Includes slugs, boosters, etc., which draw their power from the "mother" unit.
- 7-9=Not used

Second Numeric

- 0=Not used
- 1=4 axles
- 2=6 axles
- 3=8 axles
- 4=More than 8 axles
- 5-9=Not used

Third Numeric

- 0=Less than 1000 hp
- 1=1000 hp through 1499 hp
- 2=1500 hp through 1749 hp
- 3=1750 hp through 1999 hp
- 4=2000 hp through 2499 hp
- 5=2500 hp through 2999 hp
- 6=3000 hp through 3599 hp
- 7=3600 hp through 3999 hp
- 8=4000 hp through 4999 hp
- 9=5000 hp and over

EQUIPPED GONDOLA CAR TYPE CODE E _ _ _ _

First Numeric

- 1—Less than 48' inside length
- 2—Less than 48' inside length with cushion draft gear/underframe
- 3—48' and less than 52' inside length
- 4—48' and less than 52' inside length with cushion draft gear/underframe
- 5—52' and less than 61' inside length
- 6—52' and less than 61' inside length with cushion draft gear/underframe
- 7—61' and over, inside length
- 8—61' and over inside length with cushion draft gear/underframe
- 9 & 0—Not Used

Second Numeric

- 0=GTS
- 1=GTR
- 2=GBR
- 3=GBS
- 4=GBSR
- 5=GDS
- 6=GSS
- 7=Not Used
- 8=GWS

Third Numeric

- 0—All others
- 1—Coil steel/aluminum equipped car
- 2 thru 9=Not Used

FLAT CARS CAR TYPE CODE F _ _ _ _

First Numeric

- 1—Less than 111,000 lbs. nominal capacity—50 ton
- 2—111,000 lbs. and less than 155,000 lbs. nominal capacity—70 ton
- 3—155,000 lbs. and less than 201,000 lbs. nominal capacity—100 ton
- 4—200,000 lbs. and over.
- 5—Nominal capacity over 200,000 lbs. (except FM).
- 6—FM 200,000 lbs. nominal capacity and over
- 7—Equipped to handle frames (auto/truck)
- 8—Single deck, pedestals and tie downs for saddleback loading
- 9—Single deck, tie downs, without pedestals
- 9—Not Used

Second Numeric

- 0—FM
- 1—FMS, standard draft gear
- 2—FMS, equipped with cushioned draft gear/underframe
- 3—FD-FDS
- 4—FB
- 5—FBS
- 6—FW-FWS
- 7—FL
- 8 & 9—Not Used

Third Numeric

- 1—Less than 53' inside length
- 2—53' and less than 60' inside length
- 3—60' and less than 75' inside length
- 4—75' and less than 85' inside length
- 5—85' and less than 89' inside length
- 6—89' and over inside length
- 0, 7 thru 9=Not Used

UNEQUIPPED GONDOLA CAR TYPE CODE G _ _ _ _

First Numeric

- 1—Less than 48' inside length
- 2—48' and less than 52' inside length, less than 9' inside width
- 3—48' and less than 52' inside length, 9' and over inside width
- 4—52' and less than 61' inside length, less than 9' inside width
- 5—52' and less than 61' inside length, 9' and over inside width
- 6—61' and over inside length, less than 9' inside width
- 7—61' and over inside length, 9' and over inside width
- 8, 9 & 0—Not Used

Second Numeric

- 0=GA
- 1=GB steel floor, solid ends
- 2=GB steel floor, drop ends
- 3=GB wood floor, solid ends
- 4=GB wood floor, drop ends
- 5=GD
- 6=GH
- 7=Not Used
- 8=GS
- 9=Not Used

Third Numeric

- 1=sides 36" or less
- 2=sides 37" and less than 47"
- 3=sides 47" and less than 57"
- 4=sides 57" and over
- 0, 5 thru 9=Not Used

UMLER SPECIFICATION MANUAL—EXHIBIT D—CONTINUED

UNEQUIPPED HOPPER CARS

CAR TYPE CODE H — — —

First Numeric

- 1—Less than 111,000 lbs. nominal capacity—50 ton
- 2—111,000 lbs. and less than 155,000 lbs.—70 ton nominal capacity
- 3—155,000 lbs. and less than 190,000 lbs. nominal capacity—80 ton
- 4—190,000 lbs. nominal capacity and over—100 ton
- 5 thru 9—Not Used

Second Numeric

- 0—Not Used
- 1—Not Used
- 2—HFA
- 3—HK
- 4—HM
- 5—HT
- 6—HTA
- 7—Not Used
- 8—Not Used
- 9—Not Used

Third Numeric

- 0—Not Applicable
- 1 thru 9—Not Used

GONDOLA CAR—GT

CAR TYPE CODE J — — —

First Numeric

- 1—Less than 111,000 lbs. nominal capacity—50 ton
- 2—111,000 lbs. and less than 155,000 lbs.—70 ton nominal capacity
- 3—155,000 lbs. and less than 190,000 lbs. nominal capacity—80 ton
- 4—190,000 lbs. nominal capacity and over—100 ton
- 5 thru 9—Not Used

Second Numeric

- 0—GT Flat Bottom
- 1—GT Depressed Bottom
- 2 thru 9—Not Used

Third Numeric

- 0—Less than 36' inside length (Ore Jenny)
- 1—36' inside length and less than 48'
- 2—48' inside length and less than 52'
- 3—52' inside length and less than 61'
- 4—61' inside length and over
- 5 thru 9—Not Used

EQUIPPED HOPPER CARS

CAR TYPE CODE K — — —

First Numeric

- 1—Less than 111,000 lbs. nominal capacity—50 ton
- 2—111,000 lbs. and less than 155,000 lbs.—70 ton nominal capacity
- 3—155,000 lbs. and less than 190,000 lbs. nominal capacity—80 ton
- 4—190,000 lbs. nominal capacity and over—100 ton
- 5 thru 9—Not Used

Second Numeric

- 0—HKS
- 1—HMR
- 2—HMS
- 3—HTR
- 4—HTS
- 5—HKS
- 6—HTSR
- 7—HMSR
- 8—HMA (Ore Car)
- 9—NOT USED

Third Numeric

- 0—Not Applicable
- 1 thru 9—Not Used

SPECIAL TYPE CARS

CAR TYPE CODE L — — —

First Numeric

- 0—Not applicable
- 1 thru 9—Not Used

Second Numeric

- 0—LF Flat
- 1—LG Gondola
- 2—LP Flat
- 3—LPS Flat
- 4—LU Box
- 5—Not Used
- 6—LM Hopper
- 7—LC Box
- 8—Not Used
- 9—LS Flat

Third Numeric

- 0—Cubic Capacity and Length not applicable
- 1—Less than 3,000 cu. ft. capacity
- 2—3,000 but less than 4,000 cu. ft. capacity
- 3—4,000 but less than 5,000 cu. ft. capacity
- 4—5,000 cu. ft. capacity and over
- 5—Not Used
- 6—Less than 49'8" inside length
- 7—49'8" and less than 59'8" inside length
- 8—59'8" and over inside length
- 9—Not Used

MAINTENANCE OF WAY CARS—CODING

M—Maintenance of Way Cars

First and Second Numeric

- | | | | |
|----------|----------|----------|-----------|
| 10 = MW | 21 = MWS | 29 = MWH | 50 = PA |
| 11 = MWB | 23 = MWT | 30 = MWJ | 51 = PB |
| 12 = MWD | 25 = MWW | 31 = MWK | 52 = PD |
| 14 = MWE | 26 = MWX | 32 = MWL | 53 = PS |
| 15 = MWF | 27 = MWC | 33 = MWU | * 54 = PO |
| 19 = MWM | 28 = MWG | 34 = MWV | |
| 20 = MWP | | | |

Third Numeric

- Not Used

* PO is not an official mechanical designation. It represents other not classified.

CABOOSES—CODING

N = All Caboose

First and Second Numeric

- 10 = NE

Third Numeric

- Not Used

CONVENTIONAL INTERMODAL FLAT CARS (FC)

CAR TYPE CODE P — — —

If Tare Weight is greater than 33,100 Pounds

First Numeric (See Notes 1 & 2)

- 0—Single Length - Double Stack container car
- 1—Single Length - Low Level - 8' Undercarriage
- 2—Single Length - Low Level - 8½' Undercarriage
- 3—Single Length - Standard Level - 8' Undercarriage
- 4—Single Length - Standard Level - 8½' Undercarriage
- 5—Double Length - Low Level - 8' Undercarriage
- 6—Double Length - Low Level - 8½' Undercarriage
- 7—Double Length - Standard Level - 8' Undercarriage
- 8—Double Length - Standard Level - 8½' Undercarriage
- 9—Double Length Car with Deck Height 3'2" ATR - 8' Undercarriage

Second Numeric

- 1—Circus and Lift-On/Lift-Off (Not applicable to container cars).
- 2—Circus (with portable bridge plates) and Lift-On/Lift-Off (Not applicable to container cars).
- 3—Lift-On/Lift-Off only (no bridge plates & Non-retractable hitches)
- 4—0 Not used

Third Numeric (See Note 3)

If 1st Numeric is 0 thru 4 (single length), then

- 0—Cars not otherwise classified, contact owner.
- 1—Trailer up to 45'
- 2—Trailer up to 48'
- 3—Trailer up to 50'
- 4—Trailer up to 45' with nose mounted reefer
- 5—Trailer up to 48' with nose mounted reefer
- 6—Trailer up to 50' with nose mounted reefer
- 7—Container only - up to 40'
- 8—Container only - up to 45'
- 9—All purpose - Trailer or container of varying sizes

If 1st Numeric is 5 thru 9 (double length), then

- 0—Cars otherwise not classified, contact owner
- 1—2-40' trailers with or without nose mounted reefers (If 1st numeric equals 9, car will not handle nose mounted reefers).
- 2—1-40' trailer without and 1-45' trailer with nose mounted reefer, or
- 2-40' trailers with nose mounted reefer.
- 3—2-42' trailers with nose mounted reefers
- 4—2-45' trailers (see note 4)
- 5—1-40' trailer without and 1-45' trailer with nose mounted reefer, or
- 3-28' "Pups" or 2-40' trailers with nose mounted reefer.
- 6—2-40' or 4-20' containers and various combinations
- 7—2-40' or 45' containers
- 8—Trailers and/or containers as follows - 1-40' trailer without and 1-45' trailer with nose mounted reefer, or 2-40' trailers with nose mounted reefer, or various combinations of 20' and 40' containers and/or trailers. (Combination of double-length car 3rd numerics 2 & 6 above)
- 9—Trailers and/or containers as follows - 2-45' trailers without nose mounted reefers, or various combinations of 20' and 40' containers and/or trailers. (Combination of double-length car 3rd numerics 4 & 6 above) - (See Note 4)

Note 1—"Single Length" car will handle one unit at least 40' long. (Car will generally be 50' - 75' long)
"Double Length" car will handle two units at least 40' long. (Car will generally be 85' - 89'4" long)

Note 2—"Low Level" is 2'9" or less ATR
"Standard Level" is 3'4" inches ATR or over

Note 3—Although other king pin settings may be acceptable, trailer handling capabilities assume trailer king pin settings of 36".

Note 4—These cars will also handle 40' or 45' trailers with nose mounted reefer units at the "A" position (middle) hitch provided the "B" position (leading) hitch is carrying a 40' or shorter trailer. In no case will the "B" position hitch handle a trailer with nose mounted reefer regardless of the length of the trailer.

LIGHT WEIGHT, LOW PROFILE, SINGLE PLATFORM (FC) AND/OR ARTICULATED INTERMODAL CARS (FCA) CAR TYPE CODE Q — — —

Mechanical Designation "FC"—If Less Than 33M Pounds
and
"FCA" Articulated and Multi-Platform Cars
(See Notes 1-4 Below)

First Numeric

- 1—Trailers Only—8' Wide Undercarriage
- 2—Trailers Only—8½' Wide Undercarriage
- 3—Containers Only—8' Wide Single Stack
- 4—Containers Only—8½' Wide Single Stack
- 5—Trailers with 8' wide undercarriage or 8' wide containers
- 6—Trailers with 8½' wide undercarriage or 8½' wide containers
- 7—Trailers with 8' wide undercarriage or 8½' wide containers
- 8—Trailers with 8½' wide undercarriage or 8½' wide containers
- 9—Containers only—8' wide double stack
- 0—Containers only—8½' wide double stack

Second Numeric (See Note 3)

- | | | |
|---------------|---------------|------------------------|
| 1—1 Platform | 5—5 Platforms | 9—9 Platforms |
| 2—2 Platforms | 6—6 Platforms | 0—10 or more Platforms |
| 3—3 Platforms | 7—7 Platforms | |
| 4—4 Platforms | 8—8 Platforms | |

UMLER SPECIFICATION MANUAL—EXHIBIT D—CONTINUED

Third Numeric (See Note 4)

- 0—Cars otherwise not classified, contact owner
- 1—Units 40' to 45'
- 2—Units 40' to 48'
- 3—Units 40' to 50'
- 4—Units 40' to 45' with nose mounted reefer
- 5—Units 40' to 45' with nose mounted reefers at only certain positions of the car—(See Note 5)
- 6—Units 40' to 48' with nose mounted reefer
- 7—Units 40' to 50' with nose mounted reefer
- 8—Not used
- 9—Not used

- * Note 1—All "Q" class cars have a deck height of 3'6" ATR or less.
- Note 2—All "Q" class cars are suitable only for Lift-On/Lift-Off terminal handling.
- Note 3—Multiples of two or more platforms are either fully articulated or are semi-permanently coupled together and cannot be separated except at a repair track.
- Note 4—Although other king pin settings may be acceptable, trailer handling capabilities are based on a trailer king pin setting of 36"
- Note 5—See Equipment Register or contact car owner for specific handling capabilities.

REFRIGERATOR CARS CAR TYPE CODE R — — —

- First Numeric
- 1—less than 49' inside length
- 2—less than 49' inside length with cushion draft gear/underframe
- 3—49' and less than 59' inside length
- 4—49' and less than 59' inside length with cushion draft gear/underframe
- 5—59' and less than 79' inside length
- 6—59' and less than 79' inside length with cushion draft gear/underframe
- 7—79' and over, inside length
- 8—79' and over inside length with cushion draft gear/underframe
- 9 & 0 Not Used

Second Numeric

- 0—RB
- 1—RBL
- 2—RS
- 3—Not Used
- 4—Not Used
- 5—RPL
- 6—RP
- 7—RPL
- 8—RPC
- 9—Not Used

Third Numeric

- 0—Not applicable
- 1 thru 9—Not Used

STOCK CARS—CODING

- S—Stock Cars

First Numeric

- Not Used

Second Numeric

- 1—Not Used
- 2—SC
- 3—Not Used
- 4—Not Used
- 5—Not Used
- 6—SM
- 7—ST

Third Numeric

- 0—Deck Height No Factor
- 1—High Double Deck (Lower deck 5'4" and over) (SC Cars Only)
- 2—Convertible Deck (SC Cars Only)

TANK CARS—CODING

- T—Tank Cars

First and Second Numeric

Major Class Description (See Exhibit M)

Third Numeric

- 0—Capacity not applicable
- 1—7,000 gal. and less capacity
- 2—8,000 through 9,000 gallons capacity
- 3—10,000 through 11,000 gallons capacity
- 4—12,000 through 18,000 gallons capacity
- 5—19,000 through 21,000 gallons capacity
- 6—22,000 through 24,000 gallons capacity
- 7—25,000 through 27,000 gallons capacity
- 8—28,000 through 31,000 gallons capacity
- 9—32,000 gallons capacity and over

For the purpose of determining capacity for coding, the following is used:

- 6,500 to 7,499 gallons—show as 7,000 gallons capacity
- 7,500 to 8,499 gallons—show as 8,000 gallons capacity
- 8,500 to 9,499 gallons—show as 9,000 gallons capacity
- Etc.

VEHICULAR FLAT CARS (FA ONLY)

CAR TYPE CODE V — — —

First Numeric

- 0—Tri-level rack, open or non side-shielded
- 1—Tri-level rack, screened or side-shielded
- 2—Tri-level rack, partially enclosed no roof
- 3—Tri-level rack, partially enclosed no doors
- 4—Tri-level rack, fully enclosed both roof and doors
- 5—Bi-level rack, open or non side-shielded
- 6—Bi-level rack, screened or side-shielded
- 7—Bi-level rack, partially enclosed no roof
- 8—Bi-level rack, partially enclosed no doors
- 9—Bi-level rack, fully enclosed both roof and doors

Second Numeric

- 0—Any height, any length, without cushioning
- *1—Low level, platform length less than 89' 4", equipped with hinged B deck
- *2—Low level, platform length 89' 4" and over, equipped with hinged B deck
- 3—Low level, rigid rack, less than 89' 4" platform length
- 4—Low level, rigid rack, 89' 4" platform length and over
- *5—Mid level, platform length 89' 4" and over, equipped with hinged B deck
- 6—Mid level, rigid rack, 89' 4" platform length and over
- 7—High level, rigid rack, less than 89' platform length
- 8—High level, rigid rack, 89' and less than 89' 4" platform length
- 9—High level, rigid deck, 89' 4" platform length and over

NOTE: Platform heights ATR are defined: Low is less than 34", Mid is 34" and less than 40" and High is 40" and over

Third Numeric

- 0—No door
- 1—Radial, full height
- 2—Rave door, full height
- 3—Wire mesh door, full height
- 4—Tri-fold door, full height
- 5—Radial door, partial height
- 6—Wire mesh door, partial height
- 7—Bi-fold door, partial height
- 8—Tri-fold door, partial height
- 9—Other, including three piece door

NOTE: A three piece door is a partial door having a hinged top piece to fully enclose end of door

CONTAINERS

CAR TYPE CODE U — — —

First Numeric

- 0—Hopper/Bulk
- 1—Not Used
- 2—General Service (Non-equipped Dry Vans)
- 3—Flat Beds (includes removable sides, platforms and expandable)
- 4—Open Tops
- 5—Mechanical Refrigerator
- 6—Tank
- 7—Insulated
- 8—Not Used
- 9—Special Equipped Straight Floor Closed

NOTE: A three piece door is a partial door having a hinged top piece to fully enclose end of door

Second Numeric

- 0—40 ft. and less than 42 ft.
- 1—Less than 20 ft.
- 2—20 ft. and less than 27 ft.
- 3—27 ft. and less than 35 ft.
- 4—35 ft. and less than 40 ft.
- 5—45 ft. and over
- 6—42 ft. and less than 45 ft.
- 7—Not Used
- 8—Not Used
- 9—Not Used

Third Numeric

- 0—Not Used
- 1—Outside Height 8' and under
- 2—Outside Height 8'1" to 8'6" incl.
- 3—Outside Height over 8'6"
- 4—Not Used
- 5—Not Used
- 6—Not Used
- 7—Not Used
- 8—Not Used
- 9—Not Used

TRAILERS

CAR TYPE CODE Z — — —

First Numeric

- 0—Hopper/Bulk
- *1—Chassis
- 2—General Service (Non-equipped Dry Vans)
- 3—Flat Beds (includes removable sides, platforms & expandables)
- 4—Open Tops
- 5—Mechanical Refrigerators
- 6—Tank
- 7—Insulated
- 8—Drop Frames (includes Wedge Frames)
- 9—Special Equipped Straight Floor Closed

Second Numeric

- 0—40 ft. and less than 42 ft.
- 1—Less than 20 ft.
- 2—20 ft. and less than 27 ft.
- 3—27 ft. and less than 35 ft.
- 4—35 ft. and less than 40 ft.
- 5—45 ft. and less than 48 ft.
- 6—42 ft. and less than 45 ft.
- 7—48 ft. and over
- 8—Not Used
- 9—Not Used

Third Numeric

- 0—Not Used
- 1—Extreme Width 8' and under—Outside Height 12'6" and under
- 2—Extreme Width 8' and under—Outside Height over 12'6" and under 13'6"
- 3—Extreme Width 8' and under—Outside Height 13'6" and under 13'6"
- 4—Extreme Width 8' and under—Outside Height 13'6" and over
- 5—Extreme Width over 8'—Outside Height 13' and under—96" Wide Tandem
- 6—Extreme Width over 8'—Outside Height over 13'—96" Wide Tandem
- 7—Extreme Width over 8'—Outside Height 13' and over—96" Wide Tandem
- 8—Extreme Width over 8'—Outside Height over 13'—over 96" Wide Tandem
- 9—Not Used

* (Second and Third Numeric for Chassis only)

CHASSIS

Second Numeric

- 0—Not Used
- 1—Not Used
- 2—20 ft. Straight
- 3—20/24 ft. Combo
- 4—Not Used
- 5—45 ft.
- 6—40 ft. Straight
- 7—40 ft. Gossneck
- 8—40 ft. Combo (20/40)
- 9—40 ft. Tri-Purpose

Third Numeric

- 0—4'6" and under
- 1—over 4'6"
- 2 thru 9—Not Used

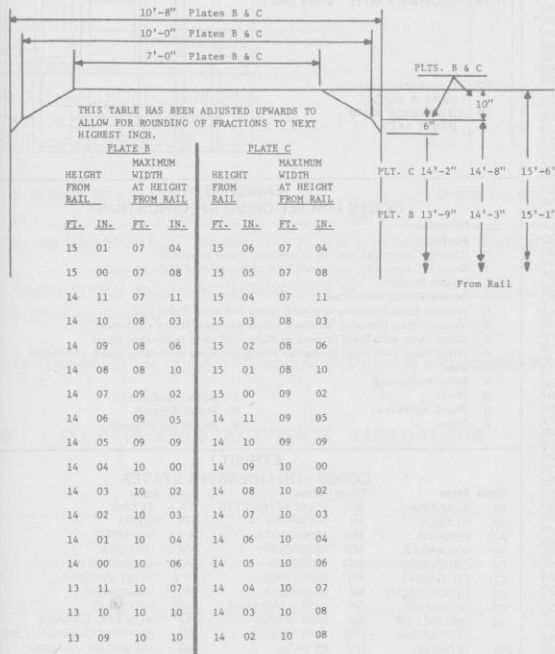
EXHIBIT E—LINING CODES

Code Name

- C Cement
- F Fiberglass (for TOFC/COFC equipment)
- G Glass
- K Kenigen
- L Sheet Metal
- M Metal Clad—Bart, Lead, Monel, Nickel, Stainless Steel
- N Composite Wood and Steel (Nailable)
- P Metal Spray Type—Aluminum, Stainless Steel, Tin, Zinc and Tin
- R Rubber—Amerplate, Rubber (Any Kind), Saran
- S Synthetic Type—Amercoat, Copon, Epoxy, Ferroglazed, Flakelass, Flomar, Freightliner, Hard Board (i.e., Masonite), Heresite, Hypalon, Kemply (for TOFC/COFC equipment), Lithcote, Placite, Plastic, Polyclutch
- U Unlined
- V Vinyl
- W Wood Lined

UMLER SPECIFICATION MANUAL—EXHIBIT F

CLEARANCES

WIDTHS AT ONE INCH INCREMENTS IN HEIGHT
TOP PORTION OF EQUIPMENT DIAGRAMS PLATES B & C
FOR UMLER EDITING CHECKS ONLY

HOW TO USE EXHIBIT F:

Ex. F's purpose is to determine if the outside ht. and width data furnished in your UMLER record is within the Plate Clearance code reported.

e.g., clearance = B, Ht. From Rail to Extreme Width = 1500, Extreme Width = 0704; you would then find the height reported (1500) under Plate B in the above table. Directly to the right of 1500 is the maximum width at that ht., in this case 07 08. Therefore, the Extreme Width reported of 07 04 is within Plate B.

In the event that the data reported for the Extreme Width in the above example was 0711, extreme width would be flagged in the error listing as follows: 07 11.

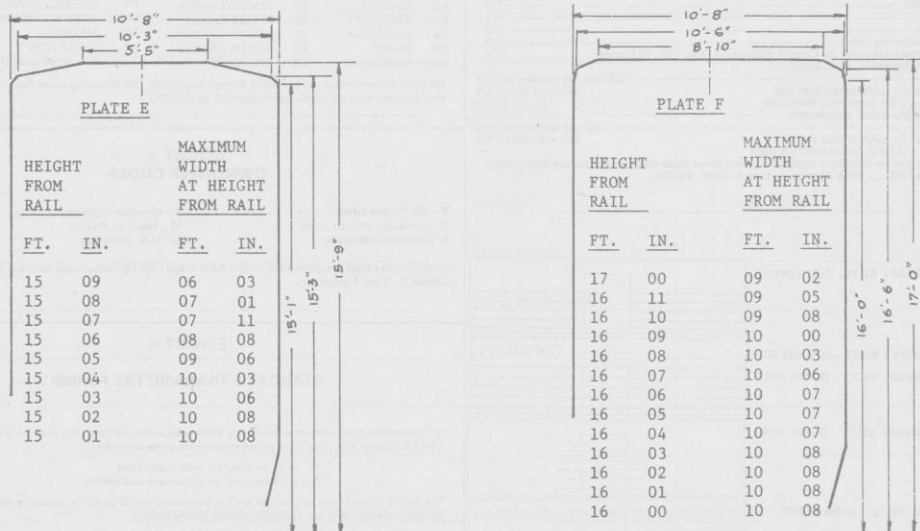
Relational errors (See Ex. L), as in the above example, indicate one of the following could be wrong:

a. 07 11 Extreme Width is not correct. It was actually 07 08 or less.

b. 15 00 ht. above rail to Extreme Width is not correct. It was actually 14 11 or less.

c. Clearance code B is incorrect, and the 2 dimensions are correct. The car is actually a Code C.

It will be the responsibility of the reporting party to resolve such errors with his mechanical department and submit the correct data.

WIDTHS AT ONE INCH INCREMENTS IN HEIGHT
TOP PORTION OF EQUIPMENT DIAGRAMS PLATES E & F
FOR UMLER EDITING ONLY

NOTE: Car dimensions in excess of Plates E and F report Code G in Clearance Field.

UMLER SPECIFICATION MANUAL—EXHIBIT G, H, I, J, K

EXHIBIT G—FITTING CODES

Adjustable Belts & Lading Strap Anchors	BA
Anchor, Lading Strap	AS
Anchor, Lading Strap, Door Only	AD
Anti-pilferage Locking Device	AP
Bulkhead, Center-beam	BC
Belt Rail—Full length (TOFC)	BR*
Bins	BN
Bows	BO
Bulkhead, Fixed Ends	BE
Bulkhead, Moveable, Single Type	BS*
Bulkhead, Moveable, Double Type	BD
Center Belt Rail-Floor Restraining Device (Z2 & Z9)	CR
Chains and Binders (applicable to FB, FC, FL and FMS)	CB
Coil Steel and Aluminum	CS
* * Combination end door hasps & straps for side panels (FA only)	DS
Come Along Door Devices (applicable car type codes A & B)	CA
Containers, Permanently attached to equipment	CN
Cover, Canvas	CC
Cover, Removable or not elsewhere classified	CV
Cradles	CD
Door Assist (Car Types A and B only)	DA
Double Deck	DD*
Double Moveable Bulkheads & Adjustable side fillers	DS
Double Moveable Bulkheads & Floor Drain	DO
* * End door hasps (applicable to FA)	DH
Fans, Floor	FF
Fans, Overhead	OF
Fiberglass Cover (HTR Types)	FC
Floor Drain (applicable to refrigerators and TOFC/COFC)	FD
Fuel Saver (applicable to RP, RPB, RPL, Z5 & U5)	FS
Heater Harness (applicable to XLI, XMI, RA, RB, RBL, RP)	HP
Heater Permanent (applicable to XLI and XMI only)	IB
Inflatable Bulkhead	FI*
Insulated Fully (TOFC)	FI
Insulated Partially (TOFC)	PI
Meat Hooks	MH
Meat Rails	MF
Non Fish-belly (Mandatory for FB, FBS, FM, FMS where applicable)	NB
Pallets	PA
Panel (sides only) (FA type)	WS
Partial Belt	PB
Perforated Sidewall	PD
Racks, Interior, Not Elsewhere Classified	RI
Rail Meat (applicable to RP and RS)	RM
Rail-wheels (TOFC)	RW
Refrigerator, Mechanical, General Purpose	GR
Refrigerator, Mechanical, Zero Only	ZR
Refrigerator, Mechanical, Not suitable for frozen foods	MR
Retention Bar (applicable to XP, XPI, XL, XLI)	RB
Rollers, in the floor	FR
Rollers, Portable Conveyor	PC
Rub Rail (applicable to XP, XPI, XL, XLI)	RR
Screens (sides only) (FA type)	SC
Side Fillers, Adjustable	AF
Side Fillers, Permanent	PF
Side Fillers, Removable	RF
Sides, Removable	RS
Single bulkhead & adjustable side fillers	SF
Stakes	ST
Stanchions	SN
Spring Tensioning Device (applicable to FB, FBS and FMS)	SD
Stationary belts & lading strap anchors	SA
* * Straps to secure side panels (applicable to FA)	SS
Tie Down Assembly	TH
Trailer Hitches (including Pintle Hook)—FC and FCA cars only	TD
Trolley Meat (RPL type)	TM
Twist Lock	TL
Vent Openings	VO
Wood Racks, covering floors	WR
Specialty equipped/designed for Class A explosives (XM, XMI, XLI types)	XA
Belts for Cross Members, Stationary:	
Doors	SB and the number of rails
Entire Side, 01 = Stationary Belt Rail	Min = 01 Max = 29
02 = Stationary Belt Rails	
Belts for Cross Members, Adjustable:	
Doors	AB and number of rails
Entire Side, 31 = Adjustable Belt Rail	Min = 31 Max = 59
32 = 2 Adjustable Belt Rails	

NOTE: If more than one code is applicable, then show each code in a separate fitting field.
* Mandatory for Z9 (See Section 3, Line 4, Cols. 24-29).

01-29 STATIONARY BELTS - FULL LENGTH

01-29 STATIONARY BELTS - FULL LENGTH

SB STATIONARY BELTS - DOOR AREA

SB STATIONARY BELTS - DOOR AREA

PB PARTIAL BELTS - A OR B END

PD PERFORATED SIDES

31-59 ADJUSTABLE BELTS - SIDES ONLY

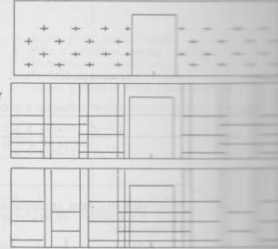
31-59 ADJUSTABLE BELTS -
AB ADJUSTABLE BELTS -
DOOR AREA

EXHIBIT H

CODES FOR FLOORING SPECIFICATIONS

Code Definition

A	Aluminum
C	Naillable Composite (will be considered same as wood)
1	Naillable Composite, reinforced (will be considered same as wood)
D	Double Wood
2	Double Wood, reinforced
N	Naillable Steel (includes alternate wood and steel floor)
3	Naillable Steel (includes alternate wood and steel floor), reinforced
P	Wood Floor with Steel Protective Plates (includes perforated steel)
4	Wood Floor with Steel Protective Plates (includes perforated steel, reinforced)
S	Steel
5	Steel, reinforced
W	Wood
6	Wood, reinforced
7	Other, reinforced
8	Wood (Ribbed)
9	Aluminum (Ribbed)
X	Other

EXHIBIT I

CODES FOR LICENSING STATES

Code	State	Code	State	Code	State
AL	ALABAMA	MA	MASSACHUSETTS	TX	TEXAS
AK	ALASKA	MI	MICHIGAN	UT	UTAH
AZ	ARIZONA	MN	MINNESOTA	VT	VERMONT
AR	ARKANSAS	MS	MISSISSIPPI	VA	VIRGINIA
CA	CALIFORNIA	MO	MISSOURI	WA	WASHINGTON
CO	COLORADO	MT	MONTANA	WV	WEST VIRGINIA
CT	CONNECTICUT	NE	NEBRASKA	WI	WISCONSIN
DE	DELAWARE	NH	NEW HAMPSHIRE	WY	WYOMING
DC	DISTRICT OF COLUMBIA	NJ	NEW JERSEY	AB	ALBERTA, CANADA
FL	FLORIDA	NM	NEW MEXICO	BC	BRITISH COLUMBIA, CANADA
GA	GEORGIA	NV	NEVADA	MB	MANITOBA, CANADA
HI	HAWAII	NY	NEW YORK	NB	NEW BRUNSWICK, CANADA
ID	IDAHOO	NC	NORTH CAROLINA	NF	NEWFOUNDLAND, CANADA
IL	ILLINOIS	ND	NORTH DAKOTA	NS	NOVA SCOTIA, CANADA
IN	INDIANA	OH	OHIO	ON	ONTARIO, CANADA
IA	IOWA	OK	OKLAHOMA	PE	PRINCE EDWARD ISLAND, CANADA
KS	KANSAS	OR	OREGON	PQ	QUEBEC, CANADA
KY	KENTUCKY	PA	PENNSYLVANIA	SK	SASKATCHEWAN, CANADA
LA	LOUISIANA	RI	RHODE ISLAND	MX	MEXICO
ME	MAINE	SC	SOUTH CAROLINA	XX	EXCEPTION
MD	MARYLAND	SD	SOUTH DAKOTA		(i.e. International TOPCODES)
		TN	TENNESSEE		

NOTE: For containers registered in foreign countries, the licensing state field is to contain two position country code, as suggested by ISO/DIS.

EXHIBIT J

OWNERSHIP CODES

B—US Private Lines	J—Mexican Railroads
C—Canadian Private Lines	M—Mexican Private
I—Canadian Railroads	U—U.S. Railroads

Note: These codes are generated by the AAR in the UMLER output tape position 252-254. See Exhibit A, Tape Formats.

EXHIBIT K

STANDARD TRANSMITTAL FORMS

A Transmittal Form should accompany each submission of data that is to be processed in the UMLER Master File. The type of submission must be identified.

1. is to be used by tape submitters.
2. is to be used by document submitters.

The use of these forms will aid the AAR in exercising better controls, expedite the processing of input and the manual handling of the output.

NOTE: The external address of the data being forwarded should be in agreement with the address of the Transmittal Form used.

UMLER SPECIFICATION MANUAL—EXHIBIT K—CONTINUED

MILLER

AMERICAN RAILROADS BUILDING

50 F ST., N.W.

WASHINGTON, DC 20001

AAR USE ONLY

UPDATE No. _____

- Enclosed find SUBMISSION NO. _____, consisting of _____ reel(s) of tape, _____
tk, _____ bpi _____, reel # _____, _____ to be processed
in the UMLER/OT-37 Master File. (Circle one or both if applicable)
- Enclosed find document SUBMISSION NO. _____, to be processed in the UMLER/OT-37 Master File. (Circle one or
both if applicable)

Indicated below are the number of records/documents and cars/trailers/containers for each mark submitted:

[illegible]

NAME _____

COMPANY _____

CITY _____

TELEPHONE () _____

COMMENTS: _____

TITLE _____

ADDRESS _____

STATE & ZIP _____

TELEX NO. _____

THIS FORM MUST ACCOMPANY ALL TAPE DOCUMENT SUBMISSIONS

UMLR SPECIFICATION MANUAL—EXHIBIT M

Major Classes of Tank Cars and AAR, ARA, DOT or ICC Container Specifications

01 MAJOR CLASS 01 ALUMINUM, NON-PRESSURE CARS AAR ICC or DOT 201A35 103ALW 201A70W 111A60ALW 211A60ALW1 111A60ALW1 211A100ALW1 111A60ALW2 111A100ALW1 111A100ALW 111A100ALW2	12 MAJOR CLASS 12 NON-PRESSURE TANK WITHIN A TANK (ALUMINUM INNER TANK) AAR ICC or DOT 206W 115A60ALW	43 MAJOR CLASS 43 STEEL PRESSURE NON-INSULATED CARS DOT 112T400F 112T400W	58 MAJOR CLASS 58 STEEL PRESSURE CARS (MULTI-UNIT TANKS) ICC or DOT ICC or DOT 106A500 106A800X 106A500X 110A500W 106A500X 110A600W 110A800W 110A100W MAJOR CLASSES 59-65, NON-HAZARDOUS MATERIALS
02 MAJOR CLASS 02 HIGH PURITY ALUMINUM, NON-PRESSURE CARS ICC or DOT 103AALW	13 MAJOR CLASS 13 NON-PRESSURE TANK WITHIN A TANK (GRADE 304 or 430 STAINLESS STEEL INNER TANK) AAR ICC or DOT 206W 115A60W6	44 MAJOR CLASS 44 STEEL PRESSURE NON-INSULATED DOT 114J340W	59 MAJOR CLASS 59 Not Used
03 MAJOR CLASS 03 BOX TANK (No ARA, AAR, DOT or ICC Specifications) XT	14 MAJOR CLASS 14 NON-PRESSURE TANK WITHIN A TANK (GRADE 304L STAINLESS STEEL INNER TANK) AAR ICC or DOT 206W 115A60W6	45 MAJOR CLASS 45 STEEL PRESSURE NON-INSULATED DOT 114S340W	60 MAJOR CLASS 60 STEEL PRESSURE NON-INSULATED CARS ICC or DOT 112A340W
04 MAJOR CLASS 04 NICKEL CARS ICC or DOT 103ANW	15 MAJOR CLASS 15 NON-PRESSURE TANK WITHIN A TANK (GRADE 316 STAINLESS STEEL INNER TANK) AAR ICC or DOT 206W 115A60W6	46 MAJOR CLASS 46 STEEL PRESSURE NON-INSULATED DOT 114T340W	61 MAJOR CLASS 61 STEEL PRESSURE NON-INSULATED CARS ICC or DOT 112A400F 112A400W
05 MAJOR CLASS 05 ACID CARS, WELDED OR RIVETED ICC or DOT 103A 103AW 103B 103B100W 103BW 111A60W2 111A60W5 111A100F2 111A100W2 111A100W5	16 MAJOR CLASS 16 NON-PRESSURE TANK WITHIN A TANK (GRADE 316L STAINLESS STEEL INNER TANK) AAR ICC or DOT 206W 115A60W6	47 MAJOR CLASS 47 STEEL PRESSURE NON-INSULATED DOT 114J400W	62 MAJOR CLASS 62 STEEL PRESSURE NON-INSULATED CARS ICC or DOT 112A500W 112J500W 112T500W
06 MAJOR CLASS 06 STAINLESS STEEL CARS (STAINLESS GRADE 304 or 430) AAR ICC or DOT 203DW 103CW 211A60W7 103DW 211A100W6 103EW 111A60W7 111A100W6	17 MAJOR CLASS 17 WOODEN TANK CARS AAR 208	48 MAJOR CLASS 48 STEEL PRESSURE NON-INSULATED DOT 114S400W	64 MAJOR CLASS 64 STEEL PRESSURE NON-INSULATED CARS ICC or DOT 114A340W
07 MAJOR CLASS 07 STAINLESS STEEL CARS (STAINLESS GRADE 304L) ICC or DOT ICC or DOT 103CW 111A60W7 103DW 111A100W6 103EW	18 MAJOR CLASS 18 STAINLESS CLAD STEEL CARS AAR ICC or DOT 203W 103AW 103W 105A300W 105A500W 111A100W1 111A100W2 111A100W3	49 MAJOR CLASS 49 STEEL PRESSURE NON-INSULATED DOT 114T400W	65 MAJOR CLASS 65 STEEL PRESSURE NON-INSULATED CARS ICC or DOT 114A400W
08 MAJOR CLASS 08 STAINLESS STEEL CARS (STAINLESS GRADE 316) ICC or DOT ICC or DOT 103CW 103EW 103DW 111A60W7 111A100W6	19 MAJOR CLASS 19 NICKEL CLAD STEEL CARS AAR ICC or DOT 203W 103AW 103W 111A100W1 111A100W2 111A100W3	50 MAJOR CLASS 50 ALUMINUM, PRESSURE CARS ICC or DOT 105A100ALW 105A200ALW 109A100ALW 109A200ALW 120A200ALW	67 MAJOR CLASS 67 PRESSURE—TANK WITHIN A TANK ICC or DOT ICC or DOT 113A60W 113A175W 113A60W2 113A120W 113C60W 113C120W 113D60W
09 MAJOR CLASS 09 STAINLESS STEEL CARS (STAINLESS GRADE 316L) ICC or DOT ICC or DOT 103CW 111A60W7 103DW 111A100W6	20 MAJOR CLASS 20 STEEL PRESSURE NON-INSULATED CARS DOT 111J100W4	51 MAJOR CLASS 51 ALUMINUM, HIGH PRESSURE CARS ICC or DOT 105A300ALW 109A300ALW	76 MAJOR CLASS 76 CRYOGENIC—TANK WITHIN A TANK AAR 204W
10 MAJOR CLASS 10 GENERAL SERVICE CARS—CARBON STEEL TANK (WELDED OR RIVETED) (INCLUDES RUBBER LINED) AAR ICC or DOT 203 103 203W 103W 211A60W1 104W 211A100W1 111A60F1 111A60W1 111A60W3 111A100F1 111A100W1 111A100W3	21 MAJOR CLASS 21 STEEL PRESSURE NON-INSULATED CARS DOT 112J340W	52 MAJOR CLASS 52 STEEL PRESSURE INSULATED CARS ARA ICC or DOT IVA 105A100W	77 MAJOR CLASS 77 HELIUM CARS** ICC or DOT 107A****
11 MAJOR CLASS 11 NON-PRESSURE TANK WITHIN A TANK (CARBON STEEL INNER TANK) AAR ICC or DOT 206W 115A60W1	22 MAJOR CLASS 22 STEEL PRESSURE NON-INSULATED CARS DOT 112J400F 112J400W	53 MAJOR CLASS 53 STEEL PRESSURE INSULATED CARS ICC or DOT 105A200F 105A200W	78 MAJOR CLASS 78 Not Used
	37 MAJOR CLASS 37 STEEL PRESSURE NON-INSULATED CARS DOT 111J100W4	54 MAJOR CLASS 54 STEEL PRESSURE CARS ICC or DOT 105A300 105A300W 109A300W 120A300W... (Applicable only when car equipped Auxiliary Feature "D", Line 2, Col. 62)	80 MAJOR CLASS 80 STAINLESS CLAD STEEL CARS DOT 105J300W 105J500W
	38 MAJOR CLASS 38 STEEL PRESSURE NON-INSULATED CARS DOT 112J340W	55 MAJOR CLASS 55 STEEL PRESSURE INSULATED CARS ICC or DOT 105A400 105A400W 120A400W... (Applicable only when car equipped Auxiliary Feature "D", Line 2, Col. 62)	81 MAJOR CLASS 81 STAINLESS CLAD STEEL CARS DOT 105S300W 105S500W
	39 MAJOR CLASS 39 STEEL PRESSURE NON-INSULATED CARS DOT 112S340W	56 MAJOR CLASS 56 STEEL PRESSURE INSULATED CARS ICC or DOT 105A500 105A500W 120A500W... (Applicable only when car equipped Auxiliary Feature "D", Line 2, Col. 62)	82 MAJOR CLASS 82 ALUMINUM PRESSURE CARS DOT 105J200ALW
	40 MAJOR CLASS 40 STEEL PRESSURE NON-INSULATED CARS DOT 112T340W	57 MAJOR CLASS 57 STEEL PRESSURE INSULATED CARS ICC or DOT 105A600W 120A600W 120J600W... (Applicable only when car equipped Auxiliary Feature "D", Line 2, Col. 62)	83 MAJOR CLASS 83 Not Used
	41 MAJOR CLASS 41 STEEL PRESSURE NON-INSULATED CARS DOT 112J400F 112J400W		84 MAJOR CLASS 84 ALUMINUM HIGH PRESSURE CARS DOT 105J300ALW
	42 MAJOR CLASS 42 STEEL PRESSURE NON-INSULATED CARS DOT 112S400F 112S400W		85 MAJOR CLASS 85 Not Used

UMLER SPECIFICATION MANUAL—EXHIBIT M—CONTINUED

86 MAJOR CLASS 86 STEEL PRESSURE INSULATED CARS DOT 105J100W	90 MAJOR CLASS 90 STEEL PRESSURE INSULATED CARS DOT 105J300W	94 MAJOR CLASS 94 STEEL PRESSURE INSULATED CARS DOT 105J500W	
87 MAJOR CLASS 87 Not Used	91 MAJOR CLASS 91 STEEL PRESSURE INSULATED CARS DOT 105S300W	95 MAJOR CLASS 95 STEEL PRESSURE INSULATED CARS DOT 105S500W	
88 MAJOR CLASS 88 STEEL PRESSURE INSULATED CARS DOT 105J200F 105J200W	92 MAJOR CLASS 92 STEEL PRESSURE INSULATED CARS DOT 105J400W	96 MAJOR CLASS 96 STEEL PRESSURE INSULATED CARS DOT 105J600W	
89 MAJOR CLASS 89 Not Used	93 MAJOR CLASS 93 STEEL PRESSURE INSULATED CARS DOT 105S400W	97 MAJOR CLASS 97 STEEL PRESSURE INSULATED CARS DOT 105S600W	
		99 MAJOR CLASS 99 Not Used	
		*NOTE: For ARA specifications, alphabetic "I" will be used in lieu of numeric one (1). *NOTE: For UMLER reporting only, report 107A, do not report the four (4) asterisks.	

UMLER SPECIFICATION MANUAL—EXHIBIT N, O, P

EXHIBIT N

Standard Rules For Reporting Units, e.g., Trailers/Containers Which Have Other Than Four Alpha and Six Numerics For Unit Identification:

- I. Reporting Mark (Alphabetic—reading from left to right)
- A. If there is an authorized reporting mark it is to be reported exactly as stenciled on the unit.
1. Do not report RELZ as REAZ, etc.
- B. If an authorized reporting mark has less than four characters do not force a "Z" or a "U" in the fourth position of the reporting mark.
1. Example: BOZ is not to be reported as BOZZ.
- C. If the reporting mark exceeds four characters only the first four should be reported. (reading from left to right)
- D. If the reporting mark contains numerics or special characters the special characters and numerics should be dropped and only the remaining characters should be reported—up to four.
1. Example: B&OZ reported as BOZ.
- If by dropping the numerics or special characters the remaining reporting mark is one character then follow the rule in I.E.1.a.
- E. If there is no stenciled reporting mark then the following rules apply:
1. In no case should the word Company, Incorporated, etc., be considered as part of the name.
- a. Report an appropriate set of reporting marks if shown in *The Official Intermodal Equipment Register* or Standard Carrier Alpha Code Directory (SCAC).
- Example: 6A 4100 American President Lines to be reported as APLS 4100.
- b. If one name and no authorized reporting mark, the first four characters of the name (reading from left to right) using initial(s) as part of the name.
- c. If two names, and no authorized reporting mark, the first two characters of the first name plus the first two characters of the second name (reading from left to right) using the initial(s) as part of the name.
- d. If three or more names, and no authorized reporting mark, the first character of the first two names and the first two characters of the third name (reading from left to right) using the initial(s) as part of the name.
- II. Unit Number (Numeric—reading from left to right)
- A. If the unit numeric is all numeric and more than six, report the first six.
- B. If the unit number field contains alpha or special characters, they should be dropped and up to six numerics should be reported.
1. Example: NYKB C1000 to be reported as NYKB 1000.
- C. If no number, report six zeroes (000000).
- The aforementioned applies to mechanized reporting from the field, to operating systems, to UMLER, to TRAIN, etc. It is recommended that the initial manually prepared document, if any, be retained without alteration so that an audit trail can be maintained. Carriers, at their option, may append the identifier reported to the system.

EXHIBIT O

Transportation Code Specifications

- *S —Condemned car or one destined for scrap or dismantling. Automatically overrides all other codes.
- *Y —Car is prohibited or restricted in interchange service by FRA Regulations. Overrides all codes, except S. (See Exhibit P).
- *X —Car is prohibited or restricted in interchange service by AAR Interchange Rules. Overrides all codes, except S and Y. (See Exhibit P).
- **F —Railroad flat car covered under the Provisions of TD No. 3. Pool Code must be blank. Must be reported by owner.
- M —Reporting mark cancelled by AAR.
- ***O —Car must be returned to owner for lease termination or repairs.
- G —Cars used for loading of contaminated commodities as provided in Car Service Rule 14. Pool Code number may be blank. If Pool Code number is not blank, pool must be Type "G".
- T —Empty Boxcars handle via SCO-90/Rule 2 (See Section V, Item M).
- *Note 1—All cars having these restrictions will be zero rated.

**Note 2—Car Type Code For TD Circular No. 3

FM	F50__	These cars must be furnished to The Official Railway Equipment Register and to the AAR Transportation Division
FD	FDS	
FW	FWS	

***Note 3—Written request from owner and AAR approval of such request are required prior to the owner's submission of this code.

Transportation Code Specifications Applicable to Assignable Cars

- B —Railroad car assigned to a specific shipper at a specific location. Equipment covered under the Provisions of CSD 165. Must be a "B" or "E" type pool.
- C —Railroad car assigned to a specific shipper at a specific location. Equipment covered under the Provisions of CSD 145 and 435. Must be a "C" or "L" type pool.
- D —Private car, leased to a railroad, assigned to a specific shipper at a specific location. Must be a "C" or "L" type pool.
- E —Cars exempt from the provisions of CSD 150, 155 or 435. FC-FCA flat car subject to provisions of SCO 110.
- G —Cars used for loading of contaminated commodities as provided in Car Service Rule 14. Pool Code number may be blank. If Pool Code number is not blank, pool must be a "G" type pool.
- H —Cars restricted to a Commodity Pool. Must be "H" type pool. Cars are covered under the Provisions of CSD 165.
- J —Car restricted to an Agent's Pool. Must be "J" type pool. Cars are covered under the Provisions of CSD 165.
- N —National Pool car assigned to a specific shipper but not to a specific point. Must be a "N" type pool. If railroad car, must be covered under the Provisions of CSD 145 or 435.
- P —Car restricted to a Commodity Pool. Must be a "P" type pool. If railroad cars, must be covered under the Provisions of CSD 145, 155 or 435.
- R —Car restricted to an Agent's Pool. Must be "R" type pool. If railroad cars, must be covered under the Provisions of CSD 145, 155 or 435.
- U —Unassigned railroad car to be handled in accordance with the applicable Provisions of CSD 150, 155.

- W —Unassigned railroad car to be handled in accordance with the applicable Provisions of CSD 435.
- X —Cars assignable to all pool types, except N—National Pool.

NOTE: See ITEM E from Section V.

Transportation Codes Applicable to TOFC/COFC

- AA —Trailer/container, designated a general service unit by owner, must be handled in accordance with Trailer Service Rule 1.
- AB —Trailer/container not designated a general service unit by owner, must be handled in accordance with Trailer Service Rule 2.
- M —Reporting mark cancelled by AAR.
- O —Unit must be returned to owner for lease termination or repairs.

EXHIBIT P

TRANSPORTATION CONDITION CODES FOR CARS AND/OR CAR PARTS RESTRICTED IN INTERCHANGE

Input Format:
Col. 19 = X for AAR
Col. 20 = One of the codes listed below

Transportation Condition

- A Age*
- B Air Brakes
- C Axles
- D Couplers and Coupler Parts
- F Coupler Yokes
- G Draft Gears
- J Journal Bearings and Journal Lubrication
- N Trucks
- P Truck Side Frame
- T Truck Bolsters
- U Tank Cars
- W Wheels
- Z Other Restriction

* This code will be inserted by the AAR and will override all other interchange restriction codes.

Input Format:
Col. 19 = Y for FRA
Col. 20 = One of the codes listed below

Transportation Condition

- A Age**
- B Air Brakes
- C Axles
- D Couplers
- F Yokes
- G Draft Arrangements
- H Duryea Underframe Constructed before April 1, 1950
- J Plain Journal Bearings
- L Roller Bearings
- N Trucks
- P Truck Side Frame
- T Truck Bolsters
- W Wheels
- Z Other Restriction

* This code will be inserted by the AAR and will override all other interchange restriction codes.

Input Format:
Col. 19 = blank, F, M, O, G or applicable Transportation Code assigned through pool system.
Col. 20 = C applicable to (Canadian & Mexican) cars subject to U.S. Customs Regulations that are considered "domestic" in the United States under Section 123.13 and Section 123.12(d) of the regulations. Submitted via UMLER Transaction.

UMLR SPECIFICATION MANUAL — EXHIBIT Q

EXHIBIT Q
Tank Car Mileage Allowances
EFFECTIVE SEPTEMBER 1, 1986

Tank Cars**

Value	MILEAGE ALLOWANCES PER EX PARTE NO. 328						
	1-5 yrs.	6-10 yrs.	11-15 yrs.	16-20 yrs.	21-25 yrs.	26-30 yrs.	31+ yrs.
\$ 1- 1000	\$0.095	\$0.149	\$0.204	\$0.269	\$0.325	\$0.279	\$0.259
1001- 2000	0.108	0.163	0.218	0.284	0.340	0.294	0.261
2001- 3000	0.122	0.177	0.232	0.298	0.355	0.309	0.262
3001- 4000	0.135	0.190	0.246	0.312	0.370	0.324	0.263
4001- 5000	0.149	0.204	0.259	0.327	0.385	0.339	0.264
5001- 6000	0.162	0.218	0.273	0.341	0.400	0.354	0.266
6001- 7000	0.176	0.231	0.287	0.355	0.415	0.369	0.267
7001- 8000	0.189	0.245	0.301	0.369	0.430	0.385	0.268
8001- 9000	0.202	0.259	0.315	0.384	0.445	0.400	0.270
9001-10000	0.216	0.273	0.329	0.398	0.460	0.415	0.271
10001-11000	0.229	0.286	0.342	0.412	0.475	0.430	0.272
11001-12000	0.243	0.300	0.356	0.427	0.490	0.445	0.273
12001-13000	0.256	0.314	0.370	0.441	0.505	0.460	0.275
13001-14000	0.270	0.327	0.384	0.455	0.520	0.475	0.276
14001-15000	0.283	0.341	0.398	0.469	0.535	0.490	0.277
15001-16000	0.297	0.355	0.412	0.484	0.550	0.505	0.279
16001-17000	0.310	0.368	0.425	0.498	0.565	0.520	0.280
17001-18000	0.324	0.382	0.439	0.512	0.580	0.535	0.281
18001-19000	0.337	0.396	0.453	0.526	0.595	0.550	0.283
19001-20000	0.351	0.409	0.467	0.541	0.609	0.565	0.284
20001-21000	0.364	0.423	0.481	0.555	0.624	0.580	0.285
21001-22000	0.378	0.437	0.495	0.569	0.639	0.595	0.286
22001-23000	0.391	0.451	0.508	0.584	0.654	0.610	0.288
23001-24000	0.405	0.464	0.522	0.598	0.669	0.626	0.289
24001-25000	0.418	0.478	0.536	0.612	0.684	0.641	0.290
25001-26000	0.432	0.492	0.550	0.626	0.699	0.656	0.292
26001-27000	0.445	0.505	0.564	0.641	0.714	0.671	0.293
27001-28000	0.459	0.519	0.578	0.655	0.729	0.686	0.294
28001-29000	0.472	0.533	0.591	0.669	0.744	0.701	0.295
29001-30000	0.486	0.546	0.605	0.684	0.759	0.716	0.297
30001-31000	0.499	0.560	0.619	0.698	0.774	0.731	0.298
31001-32000	0.513	0.574	0.633	0.712	0.789	0.746	0.299
32001-33000	0.526	0.587	0.647	0.726	0.804	0.761	0.301
33001-34000	0.539	0.601	0.661	0.741	0.819	0.776	0.302
34001-35000	0.553	0.615	0.674	0.755	0.834	0.791	0.303
35001-36000	0.566	0.629	0.688	0.769	0.849	0.806	0.305
36001-37000	0.580	0.642	0.702	0.784	0.864	0.821	0.306
37001-38000	0.593	0.656	0.716	0.798	0.879	0.836	0.307
38001-39000	0.607	0.670	0.730	0.812	0.893	0.851	0.308
39001-40000	0.620	0.683	0.744	0.826	0.908	0.866	0.310
40001-41000	0.634	0.697	0.757	0.841	0.923	0.882	0.311
41001-42000	0.647	0.711	0.771	0.855	0.938	0.897	0.312
42001-43000	0.661	0.724	0.785	0.869	0.953	0.912	0.314
43001-44000	0.674	0.738	0.799	0.884	0.968	0.927	0.315
44001-45000	0.688	0.752	0.813	0.898	0.983	0.942	0.316
45001-46000	0.701	0.765	0.827	0.912	0.998	0.957	0.317
46001-47000	0.715	0.779	0.840	0.926	1.013	0.972	0.319
47001-48000	0.728	0.793	0.854	0.941	1.028	0.987	0.320
48001-49000	0.742	0.807	0.868	0.955	1.043	1.002	0.321
49001-50000	0.755	0.820	0.882	0.969	1.058	1.017	0.323
50001-51000	0.769	0.834	0.896	0.983	1.073	1.032	0.324
51001-52000	0.782	0.848	0.910	0.998	1.088	1.047	0.325
52001-53000	0.796	0.861	0.923	1.012	1.103	1.062	0.327
53001-54000	0.809	0.875	0.937	1.026	1.118	1.077	0.328
54001-55000	0.823	0.889	0.951	1.041	1.133	1.092	0.329
55001-56000	0.836	0.902	0.965	1.055	1.148	1.107	0.330
56001-57000	0.850	0.916	0.979	1.069	1.162	1.123	0.332
57001-58000	0.863	0.930	0.993	1.083	1.177	1.138	0.333
58001-59000	0.876	0.944	1.006	1.098	1.192	1.153	0.334
59001-60000	0.890	0.957	1.020	1.112	1.207	1.168	0.336
60001-61000	0.903	0.971	1.034	1.126	1.222	1.183	0.337
61001-62000	0.917	0.985	1.048	1.141	1.237	1.198	0.338
62001-63000	0.930	0.998	1.062	1.155	1.252	1.213	0.339
63001-64000	0.944	1.012	1.076	1.169	1.267	1.228	0.341
64001 and over	0.957	1.026	1.089	1.183	1.282	1.243	0.342

Note: Maximum Value for cars built new or rebuilt are as follows:

\$64001—1982 and prior
\$58001—1983
\$56001—1984
\$51001—1985 and subsequent
\$54001—1986

NOTE: Age is determined by subtracting the year of construction (built/rebuilt) from the current calendar year plus 1, e.g. [1984-1972]+1 = 13

Covered Hopper Cars**

LO Rates, EFFECTIVE FEBRUARY 1, 1983
Revised (Rounding) 9/1/82

Cost Bracket	*Year 1-30	*31 Years & Over
\$ 0- 1000	\$.101	\$.097
1001- 2000	.109	.098
2001- 3000	.117	.098
3001- 4000	.126	.099
4001- 5000	.134	.100
5001- 6000	.143	.101
6001- 7000	.151	.101
7001- 8000	.159	.102
8001- 9000	.168	.103
9001-10000	.176	.104
10001-11000	.185	.105
11001-12000	.193	.105
12001-13000	.201	.106
13001-14000	.210	.107
14001-15000	.218	.108
15001-16000	.227	.108
16001-17000	.235	.109
17001-18000	.243	.110
18001-19000	.252	.111
19001-20000	.260	.112
20001-21000	.269	.112
21001-22000	.277	.113
22001-23000	.285	.114
23001-24000	.294	.115
24001-25000	.302	.115
25001-26000	.311	.116
26001-27000	.319	.117
27001-28000	.327	.118
28001-29000	.336	.119
29001-30000	.344	.119
30001-31000	.353	.120
31001-32000	.361	.121
32001-33000	.369	.122
33001-34000	.378	.122
34001-35000	.386	.123
35001-36000	.394	.124
36001-37000	.403	.125
37001-38000	.411	.125
38001-39000	.420	.126
39001-40000	.428	.127
40001-41000	.436	.128
41001-42000	.445	.129
42001-43000	.453	.129
43001-44000	.462	.130
44001-45000	.470	.131
45001-46000	.478	.132
46001-47000	.487	.132
47001-48000	.495	.133
48001-49000	.504	.134
49001-50000	.512	.135
50001-51000	.520	.136
51001-52000	.529	.136
52001-53000	.537	.137
53001 and over	.546	.138

* Effective August 1, 1983

\$.006 mills	— GB and all other freight cars not indicated
.015	— GA, GD, GH, GS, GT, GW, LP
.023	— SC, SM
.038	— HFA, HK, HM, HMA, HT, HTA
.040	— FD, FM*, FW
.050	— RS
.055	— LG, LM
.120	— LF
.282	— LU** (\$.322 Short Route Mile)
.275	— RB, RBL** (\$.313 Short Route Mile)
.120	— RP, RPL
.240	— RPC**
.376	— FMS of 280,000 pounds and over nominal capacity, equipped with chain tie-down devices and/or spring tensioning device. (Fitting codes CB and SD), (effective 4/1/82).

NOTE 1: Mileage rates specified will apply on cars stenciled with the suffix R and/or S.

NOTE 2: Age is determined by subtracting the year of construction (built/rebuilt) from the current calendar year plus 1, e.g. [1984-1972]+1 = 13

* — Cars must have a nominal capacity of 200,000 pounds and over.

** — Mileage rates for loaded miles only.

Mileage rates for loaded and empty miles.

UMLR SPECIFICATION MANUAL—EXHIBIT R, S, T, U

EXHIBIT R
APPORTMENT RATE TABLE S—HOURLY TIME CHARGE
REFERENCE CAR HIRE RULE 1 (a) (4)

ORIGINAL COST	YEAR 1	YEAR 2	YEAR 3	YEAR 4	YEAR 5	YEAR 6	YEAR 7	YEAR 8	YEAR 9	YEAR 10	YEAR 11	YEAR 12	YEAR OVER 12
00000-01000	0.11	0.11	0.11	0.11	0.11	0.11	0.11	0.11	0.11	0.10	0.10	0.10	0.09
01001-02000	0.14	0.14	0.14	0.13	0.13	0.13	0.13	0.12	0.12	0.12	0.12	0.12	0.09
02001-03000	0.16	0.16	0.16	0.15	0.15	0.15	0.14	0.14	0.14	0.13	0.13	0.13	0.09
03001-04000	0.19	0.19	0.19	0.18	0.18	0.17	0.17	0.16	0.16	0.15	0.15	0.15	0.09
04001-05000	0.22	0.21	0.20	0.20	0.19	0.19	0.19	0.18	0.18	0.17	0.17	0.16	0.15
05001-06000	0.24	0.23	0.23	0.22	0.21	0.21	0.20	0.19	0.19	0.18	0.17	0.17	0.09
06001-07000	0.27	0.26	0.26	0.25	0.24	0.24	0.23	0.22	0.21	0.20	0.20	0.19	0.18
07001-08000	0.29	0.28	0.27	0.27	0.26	0.25	0.24	0.23	0.22	0.21	0.20	0.19	0.18
08001-09000	0.32	0.31	0.30	0.29	0.28	0.27	0.26	0.25	0.24	0.23	0.22	0.21	0.10
09001-10000	0.34	0.33	0.32	0.31	0.30	0.29	0.28	0.26	0.25	0.24	0.23	0.22	0.10
10001-11000	0.37	0.36	0.34	0.33	0.32	0.31	0.29	0.28	0.27	0.26	0.25	0.23	0.10
11001-12000	0.39	0.38	0.37	0.35	0.34	0.33	0.31	0.30	0.29	0.27	0.26	0.25	0.10
12001-13000	0.42	0.40	0.39	0.38	0.36	0.35	0.33	0.32	0.30	0.29	0.27	0.26	0.10
13001-14000	0.44	0.43	0.41	0.40	0.38	0.37	0.35	0.34	0.32	0.30	0.29	0.27	0.10
14001-15000	0.47	0.45	0.44	0.42	0.40	0.39	0.37	0.35	0.34	0.32	0.30	0.29	0.10
15001-16000	0.49	0.48	0.46	0.44	0.42	0.41	0.39	0.37	0.35	0.34	0.32	0.30	0.10
16001-17000	0.52	0.50	0.48	0.46	0.44	0.43	0.41	0.39	0.37	0.35	0.33	0.31	0.10
17001-18000	0.55	0.53	0.51	0.49	0.47	0.45	0.43	0.41	0.39	0.37	0.35	0.33	0.10
18001-19000	0.57	0.55	0.53	0.51	0.49	0.47	0.44	0.42	0.40	0.38	0.36	0.34	0.10
19001-20000	0.60	0.57	0.55	0.53	0.51	0.49	0.46	0.44	0.42	0.40	0.37	0.35	0.10
20001-21000	0.62	0.60	0.58	0.55	0.53	0.51	0.48	0.46	0.44	0.41	0.39	0.37	0.11
21001-22000	0.65	0.62	0.60	0.57	0.55	0.53	0.50	0.48	0.45	0.43	0.40	0.38	0.11
22001-23000	0.67	0.65	0.62	0.60	0.57	0.55	0.52	0.49	0.47	0.44	0.42	0.39	0.11
23001-24000	0.70	0.67	0.64	0.62	0.59	0.56	0.54	0.51	0.49	0.46	0.43	0.41	0.11
24001-25000	0.72	0.70	0.67	0.64	0.61	0.58	0.56	0.53	0.50	0.47	0.45	0.42	0.11
25001-26000	0.75	0.72	0.69	0.66	0.63	0.60	0.58	0.55	0.52	0.49	0.46	0.43	0.11
26001-27000	0.77	0.74	0.71	0.68	0.65	0.62	0.59	0.56	0.53	0.50	0.47	0.44	0.11
27001-28000	0.80	0.77	0.74	0.71	0.68	0.64	0.61	0.58	0.55	0.52	0.49	0.46	0.11
28001-29000	0.82	0.79	0.76	0.73	0.70	0.66	0.63	0.60	0.57	0.54	0.50	0.47	0.11
29001-30000	0.85	0.81	0.78	0.75	0.72	0.68	0.65	0.62	0.59	0.55	0.52	0.48	0.11
30001-31000	0.88	0.84	0.81	0.77	0.74	0.70	0.67	0.63	0.60	0.57	0.53	0.50	0.11
31001-32000	0.90	0.87	0.83	0.79	0.76	0.72	0.69	0.65	0.62	0.58	0.55	0.51	0.11
32001-33000	0.93	0.89	0.85	0.82	0.78	0.74	0.71	0.67	0.63	0.60	0.56	0.52	0.12
33001-34000	0.95	0.91	0.88	0.84	0.80	0.76	0.73	0.69	0.65	0.61	0.57	0.54	0.12
34001-35000	0.98	0.94	0.90	0.86	0.82	0.78	0.74	0.71	0.67	0.63	0.59	0.55	0.12
35001-36000	1.00	0.96	0.92	0.88	0.84	0.80	0.76	0.72	0.68	0.64	0.60	0.56	0.12
36001-37000	1.03	0.99	0.95	0.90	0.86	0.82	0.78	0.74	0.70	0.66	0.62	0.58	0.12
37001-38000	1.05	1.01	0.97	0.93	0.88	0.84	0.80	0.76	0.72	0.67	0.63	0.59	0.12
38001-39000	1.08	1.04	0.99	0.95	0.91	0.86	0.82	0.78	0.73	0.69	0.65	0.60	0.12
39001-40000	1.10	1.06	1.02	0.97	0.93	0.88	0.84	0.79	0.75	0.70	0.66	0.62	0.12
40001-41000	1.13	1.08	1.04	0.99	0.95	0.90	0.86	0.81	0.77	0.72	0.67	0.63	0.12
41001-42000	1.15	1.11	1.06	1.02	0.97	0.92	0.88	0.83	0.78	0.74	0.69	0.64	0.12
42001-43000	1.18	1.13	1.08	1.04	0.99	0.94	0.89	0.85	0.80	0.75	0.70	0.66	0.12
43001-44000	1.21	1.16	1.11	1.06	1.01	0.96	0.91	0.86	0.82	0.77	0.72	0.67	0.12
44001-45000	1.23	1.18	1.13	1.08	1.03	0.98	0.93	0.88	0.83	0.78	0.73	0.68	0.12
45001-46000	1.26	1.21	1.15	1.10	1.05	1.00	0.95	0.90	0.85	0.80	0.75	0.70	0.13
46001-47000	1.28	1.23	1.18	1.13	1.07	1.02	0.97	0.92	0.86	0.81	0.76	0.71	0.13
47001-48000	1.31	1.25	1.20	1.15	1.09	1.04	0.99	0.93	0.88	0.83	0.77	0.72	0.13
48001-49000	1.33	1.28	1.22	1.17	1.12	1.06	1.01	0.95	0.90	0.84	0.79	0.73	0.13
49001-50000	1.36	1.30	1.25	1.19	1.14	1.08	1.03	0.97	0.91	0.86	0.80	0.75	0.13
50001-51000	1.38	1.33	1.27	1.21	1.16	1.10	1.04	0.99	0.93	0.87	0.82	0.76	0.13
51001-52000	1.41	1.35	1.29	1.24	1.18	1.12	1.06	1.00	0.95	0.89	0.83	0.77	0.13
52001-53000	1.43	1.38	1.32	1.26	1.20	1.14	1.08	1.02	0.96	0.90	0.85	0.79	0.13
53001-54000	1.46	1.40	1.34	1.28	1.22	1.16	1.10	1.04	0.98	0.92	0.86	0.80	0.13
54001-55000	1.49	1.42	1.36	1.30	1.24	1.18	1.12	1.06	1.00	0.94	0.87	0.81	0.13
55001-56000	1.51	1.45	1.39	1.32	1.26	1.20	1.14	1.08	1.01	0.95	0.89	0.83	0.13
56001-57000	1.54	1.47	1.41	1.35	1.28	1.22	1.16	1.09	1.03	0.97	0.90	0.84	0.13
57001-58000	1.56	1.50	1.43	1.37	1.30	1.24	1.18	1.11	1.05	0.98	0.92	0.85	0.14
58001-59000	1.59	1.52	1.46	1.39	1.32	1.26	1.19	1.13	1.06	1.00	0.93	0.87	0.14
59001-60000	1.61	1.55	1.48	1.41	1.35	1.28	1.21	1.15	1.08	1.01	0.95	0.88	0.14

NOTE: Even dollars only - raise all cents to next dollar.
EFFECTIVE: July 1, 1978

For Hourly and Mileage Car Hire Rate Table See AAR Circular No. OT-10, Appendix R

Effective: November 4, 1980
EXHIBIT S—FLEXIBLE CAR HIRE

A. Reporting Flexible Car Hire to the UMLR File

1. To apply (add/change) as a Flexible Car Hire Rate using a complete tape format report: Flexible rate indicator code 5, Line 1, Col. 61; and, the flexible rate in Line 3, Cols. 59-62 (time) and Cols. 63-66 (mileage). The flexible rate, either time or mileage or both, must be less than the prescribed car hire rate or zero. Either the time or mileage charges can be equal to the prescribed car hire rate but not both. See Note 1.
2. To apply (add/change) a Flexible Car Hire Rate via the Select Field Format, Exhibit B, submit rate indicator code 5, reference Line 1, Col. 61; and, the flexible car hire rate reference Line 3, Col. 59-62 (time) and Cols. 63-66 (mileage) in the appropriate Data Change fields. See Note 1.

B. To Remove a Car From Flexible Car Hire

1. In a complete Line 1, Col. 61, report the applicable rate indicator. See Note 2.
2. In a Select Field Format (Exhibit B) reference Line 1, Col. 61, report the applicable rate indicator in the appropriate Data Change field. See Note 2.
3. In a complete tape format, report the appropriate rate indicator position 61 and zeroes in positions 183-190. See Note 2.

NOTE 1: When changing an existing flexible car hire rate, report both the time and mileage charges. The indicator 5, for complete line or select field formats, does not have to be reported.

NOTE 2: The reporting of the appropriate rate indicator removes the flexible rate.

NOTE 3: Flexible car hire rates are not applicable for cars with transportation codes S, X or Y.

NOTE 4: Select field format changes can be submitted to other data fields without effecting the flexible rate indicator or rates.

NOTE 5: Flexible car hire is applicable to railroad-owned cars covered under Circular OT-37 Series, however, the total time charge must not be equal to or greater than the total Ex Parte 334 rate and the OT-37 supplemental rate unless only the mileage charge is being reduced in which case the time charge can be equal to the sum of both.

EXHIBIT T

A. Procedures for registering equipment with weights and/or dimensions outside the normal UMLR edits in the Exception Control File

1. Owner must submit a letter to the Director TRAIN and UMLR Services, AAR listing the car initials and numbers and the specific dimensions for applicable fields outside the UMLR edit parameters. Exception records must be submitted to the AAR at least 5 working days prior to submission of UMLR File record.
 2. Car does not have to be on the UMLR file prior to being reported to the Exception Control File and as many exception fields as necessary may be reported per car.
 3. Owners may request a list of their equipment on the Exemption Control File.
- B. The AAR will process the record as follows:
1. The AAR will key the data and construct an Exception Record.
 2. Data on an UMLR transaction that does not match the data in the Exemption Control File, will have the normal edits applied and UMLR record will be flagged in error.
 3. An UMLR transaction for equipment that is outside the edit parameters that matches the Exception Control File will be accepted as a valid record.
 4. If a car with an exception record is deleted from the UMLR file the AAR will automatically delete the Exception Control record.

C. LIST OF UMLR FIELDS ELIGIBLE FOR THE EXCEPTION CONTROL FILE

- | | |
|-----------------------------|---|
| 1. Actual Cubic Capacity | 18. Side Door Height |
| 2. Nominal Capacity | 19. Side Door Width |
| 3. Tare Weight | *20. Truck Center Length |
| 4. End Door Width | *21. Well/Depression Top Width |
| 5. End Door Height | *22. Well/Depression Top Length |
| 6. Inside Length | *23. Well/Depression Bottom Width |
| 7. Inside Width | *24. Well/Depression Bottom Length |
| 8. Inside Height | 25. Inside Platform Length |
| 9. Outside Length | 26. Inside Platform Width |
| 10. Outside Width | 27. Platform Height Above Rail |
| 11. Extreme Width | *28. Bulkhead/Container Top Width |
| 12. Height of Extrame Width | *29. Bulkhead/Container Height Above Platform |
| 13. Upper Eaves Width | *30. Inset Stake Pockets Platform Width |
| 14. Upper Eaves Height | *31. Inset Stake Pockets Platform Length |
| 15. Lower Eaves Height | *32. Center of Gravity |
| 16. Lower Eaves Width | 33. Top Deck Height |
| 17. Extreme Height | |

* NOTE: Not applicable to trailers and containers.

EXHIBIT U
UMLR ERROR CODES

ERROR CODE	EXPLANATION
01	The "from" or "to" pool number was not numeric or was not in the AAR pool header file—record is rejected.
02	For other than national pools, the submitting road was not the pool operator, reporter or owner/lessee—record is rejected.
03	Shipper name was blank.
04	Car is advanced reported. Warning only.
05	The lessee's initial was not in the AAR table of valid marks—record is rejected.
06	Car was not eligible for assignment according to Car Service Rules.
07	The submitting road was not the car owner or the operator of the "from" pool.
08	Not used.
09	The "to" pool was a national pool, but the submitting road was not the car owner.
10	Operator #4 was not on AAR table of valid operators.
11	C-Pool Type Code was not blank or 'C' or 'B'.
12	G-Pool Type Code was not blank or 'G'.
13	L-Pool Type Code was not blank or 'L' or 'E'.
14	N-Pool Type Code was not blank or 'N'.
15	T-Pool Type Code was not blank or 'T' or 'J'.
16	P-Pool Type Code was not blank or 'P' or 'H'.
17	Either no valid Pool Type Code, or more than one valid Pool Type Code.
18	Maintenance type code invalid.
19	Pool number did not begin with submitter's Rule 260 number.
20	State was blank or invalid.
21	Loading station was blank.
22	Operator #1 was blank or not on AAR table of valid operators.
23	Operator #2 was not on AAR table of valid operators.
24	Operator #3 was not on AAR table of valid operators.
25	For an add, the participant was already in the list.
26	For a delete, the participant was not in the list.
27	For an add, the maximum number of participants is 50. The record was rejected.
28	Flexible Per-diem date error. The effective date in this transaction conflicts with the effective date of the current or pending rate. Record was rejected.
29	Flexible Per-diem rate error, either the time or mileage (or both) rates in the transaction exceeded the normal rate for this car. Record was rejected.
30	Minimum Rate was inserted.
31	The CAR HIGH NUMBER was not greater than or equal to CAR LOW NUMBER. The record was rejected.
32	The car series contained more than the maximum number allowed (1500) for a series. The record was rejected. Break the series into several smaller ones, each of which contains 1500 or fewer cars.
33	The record was determined to be identical to another record. The other record was processed and this record was not rejected. The occurrence of this error may indicate a multiple submission problem.

UMLER SPECIFICATION MANUAL—EXHIBIT U CONTINUED, V

- 34 The record identified had the same key (car initial, car number, date/time submitted and transaction code) as another record but the data content was different. Both records were rejected because the sequence of the activity could not be determined.
- 35 Not used.
- 36 The record contained a line image having the same key (car initial, car number, date/time received, line number, transaction code, message number and submitter) as another record, but different data. Both records were rejected because the sequence of the activity could not be determined.
- 37 A code was encountered that was not valid according to the UMLER Specification Manual. The entire group of records was not processed.
- 38 The data did not apply to the format submitted. For example, a user attempts to change a car from one format to another without submitting a "complete" format record.
- 39 Either the CAR LOW NUMBER or CAR HIGH NUMBER (or both) was not numeric. The record was rejected.
- 40 The calculated difference between CAR LOW NUMBER and CAR HIGH NUMBER does not match what was submitted. The record was rejected.
- 41 The Passkey record contained a passkey which was not valid for the submitter's mark, or the passkey was missing. All records were rejected.
- 42 The UMLER Rate Indicator contained an '8' but the OT-37 file contained an '8' in the Estimated/Actual Indicator. The UMLER Rate Indicator was changed to match the OT-37 file, after the transaction was processed.
- 43 The UMLER Rate Indicator contained an '8' but the OT-37 file contained a '9' in the Estimated/Actual Indicator. The UMLER Rate Indicator was changed to match the OT-37 file, after the transaction was processed.
- 44 The select-field LINE/FIELD NUMBER did not constitute a valid combination for the format of the record. The record was rejected.
- 45 A delete record was submitted for a car which is not on the file. The record was rejected.
- 46 An add record was submitted for a car which is already on the file. The record was rejected.
- 47 A change record was submitted for a car which is not on the file. The record was rejected.
- 48 The line code in the record was not valid. The record was rejected.
- 49 The transaction code in the record was not recognized (not in the UMLER Specification Manual). The record was rejected.
- 50 The record was rejected due to an UMLER Edit error.
- 51 The record was accepted but contained an UMLER Edit error. If the error is not corrected within 30 days, the record will be zero-rated.
- 52 Reserved for AAR internal use only.
- 53 The record length was invalid for Tele-processing submissions. Actual record length received by AAR will be found in a four character field in positions 275 through 278 of the UMLER70 pre-edits error record.
- 54 Record was not rejected, but due to errors, was zero rated.
- 55 Car was zero rated due to overage, restriction code, etc. This message is for notification purposes only and does not indicate errors in the record.
- 56 Car initial was not in the AAR table of valid marks. The record was rejected.
- 57 The UMLER Rate Indicator contained an '8' or '9' but had no corresponding OT-37 record on file. The rate indicator was changed to a '7'.
- 58 Car was already assigned to the 'to' pool. This is an UMLER77 notification of unnecessary activity. No updating has occurred.
- 59 Car(s) was zero rated due to errors in UMLER record for more than 30 days.
- 60 The record was rejected because the car cannot be assigned with a Transportation Code E, M, or O.
- 61 Value of 'EST' encountered in weighing road field for a non-advance registered car.
- 62 The UMLER Rate Indicator contained an '8', '9', or 'A' but the car was rebuilt subsequent to the cars rehabilitation. The rate indicator was changed to a '7' and the surcharge was not added to the hourly rate.
- 63 Not used.
- 64 The UMLER Rate Indicator contained an 'A' but the OT-37 file contained an '8' in the Estimated/Actual Indicator. The UMLER Rate Indicator was changed to match the OT-37 file, after the transaction was processed.
- 65 Not used.
- 66 Special Pool Activity (9999001, 9999002, or 9999003) are in error. Record was rejected as a pre-edit error.
- 67 Car with a 'G' Transportation Code cannot be assigned to other than 'G' (contaminated) Type Pool.
- 68 No TRAIN activity has occurred on a car in one (1) year. Transportation Code '2C' has been assigned to car.
- 69 No TRAIN activity has occurred on a car in two (2) years. Transportation Code '2D' has been assigned to car. In thirty (30) days this car will be deleted from the UMLER File.
- 70 Unit number of Railroad owned/controlled Trailer or Container is not in accordance with Section 3, Rule 1(a), (b) and (c) of the Code of Trailer and Container Service Rules.

EXHIBIT V
OT-37 REPORTING PROCEDURES
APPLICABLE ONLY TO RAILROAD OWNED/LEASED CARS

NOTES:

- A car being registered or changed with a rate indicator of 8, 9 or A, Line 1, Column 61 must have a corresponding Line 5 record registered in the OT-37 File. An OT-37 candidate car must be less than 21 years of age (original built date) at the time of outshopping and the sum of the Total Repairs (Line 5, Cols. 30-35) and Total Capitalized Costs (Line 5, Cols. 46-51) must be equal to at least 25 percent of the original cost to the original owner (Line 1, Cols. 43-48).
- An OT-37 record which subsequently qualifies for rebuilt status cannot have a rate indicator of 8, 9 or A. The record must be removed from the OT-37 File and a rate indicator other than a 8, 9 or A must be applied to the UMLER File (Line 1, Col. 61).
- A car that is registered in rebuilt status can qualify subsequently as an OT-37 candidate.

4. The AAR will apply a rate indicator of "7" to the UMLER File (Line 1, Col. 61) when:

- UMLER and OT-37 records do not match.
- An OT-37 record becomes overage.

Once a rehabilitated car is over thirty (30) years of age, the car must be removed from the OT-37 control file. Capitalized A's and B's (Line 5, Cols. 52-55) that were not included in the UMLER Ledger Value (Line 1, Cols. 53-55) because of the OT-37 application can then be added to the total A's and B's on Line 1, Cols. 60-64. This process will be performed by the AAR.

TYPE: All Cars (Except Locomotives and TOFC/COFC)

FORMAT: A, B, C and F

LINE: 5

LINE	COLUMN NO.	FIELD DESCRIPTION
5	1	LINE NUMBER

Must always be a 5.

5	2	TRANSACTION CODE
Numeric.		1 = Delete 2 = Add 3 = Change

Note: A '3' (change) code will be handled as a '2' (add) if the car is not on the OT-37 Control File.

5	3-18	CAR INITIAL AND NUMBER
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See Section 1, Format A, Line 1

5	19	INDICATOR
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A one position numeric field:

- Car subject to car hire (time/mileage) under the provisions of Circular OT-37 series 8000. Estimated costs. Not applicable to rebuilt cars.
- Car subject to car hire (time/mileage) under the provisions of Circular OT-37 series 8000. Actual costs. Not applicable to rebuilt cars.

REHABILITATION COST-FREIGHT CAR REPAIRS, ICC ACCOUNT 415

NOTE: The costs are to be reported ONLY if handled as freight car operating repair costs.

5	20-24	WAGES
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Numeric. Report the total operating portion of the wages amount involved in the rehabilitation shopping of the car. The direct wages portion of the defect amount, if any, is not to be included here.

5	25-29	MATERIALS AND SUPPLIES
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Numeric. Report the operating portion of the materials and supplies amount (excluding shop and store expense) involved in the rehabilitation shopping of the car. The materials and supplies portion of the defect amount, if any, is not to be included.

5	30-35	TOTAL FREIGHT CAR REPAIRS
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Numeric. This amount to include:

- Overhead, calculated to not exceed 120 percent of direct wages (Columns 30-35) and
- Additives as defined in AAR Interchange Rule (Rule 92, Office Manual).

TOTAL CAPITALIZED REHABILITATION COSTS

NOTE: Report only if capitalized at the time the car is rehabilitated and placed in service. Both kinds of rehabilitation can be done in a single shopping. However, to be reported on the OT-37 series valuation, they must be identified separately.

5	36-40	CAPITALIZED WAGES
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Numeric. Report the capitalized portion of the wages amount of an AAR Interchange Rule OT-37 shopping record.

5	41-45	CAPITALIZED MATERIALS & SUPPLIES
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Numeric. Report the capitalized portion of the materials and supplies amount of an AAR Interchange Rule OT-37 shopping record.

5	46-51	TOTAL CAPITALIZED COSTS
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Numeric. This amount to include:

- Overhead, calculated to not exceed 120 percent of capitalized wages (Columns 36-40) and
- Additives as defined in AAR Interchange Rule (Rule 92, Office Manual).

The Total Capitalized Costs value is calculated as follows: Match the reported year/month of the rehabilitated car to the OT-37 Total Repairs Table to determine the appropriate factor. Multiply the Capitalized Wages (Columns 36-40) by the factor to determine the product to the Capitalized Materials and Supplies Amount (Columns 41-45). Report the sum of the Total Capitalized Costs (Columns 46-51).

UMLER SPECIFICATION MANUAL—EXHIBIT V CONTINUED

5	52-56	TOT. ACTUAL ADDITION & BETTERMENT
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Numeric. Report total costs of actual Additions and Betterments at time of rehabilitation. The value reported for this field may be equal to but cannot exceed the Total Capitalized Cost amount, Columns 46-51.

This field may be blank. If not blank, the amount entered will be added to the total A & B Amount, Line 1, Columns 50-54, when a rehabilitated car reaches thirty (30) years of age. After thirty (30) years of age, supplemental car hire is eliminated and the valuation of the car for car hire purposes will be the original ledger value to the original owner, plus A's and B's.

5	57-60	REHABILITATION DATE-MONTH & YEAR
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Numeric. Report month (Columns 57-58) and year (Columns 59-60) the rehabilitated car was released from the OT-37 shopping.

5	61	P OR N INDICATOR
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Alphabetic. Report Positive (P) and Negative (N) applying to prior additions or betterments, Columns 62-66. If both (P) and (N) A's and B's exist, the indicator reported is to reflect the net value capitalized.

5	62-66	PRIOR ADDITIONS AND BETTERMENTS
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Numeric. Report the total cost of prior additions and betterments (if applicable). If Column 61, P or N Indicator is blank, so must this field be blank.

5	67-71	FACTORED PRIOR ADDITIONS & BETTERMENTS
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Numeric. For each prior A & B reported in Columns 62-66, match its individual release to service date to the AAR's Factorized A & B Table. Sum and report the total factorized A & B costs.

5	72-75	CAR TYPE CODE
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Alpha/Numeric. Reflects the AAR Car Type Code at the time of rehabilitation.

5	76-80	BLANK
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APPENDICES:
The following "protected" fields are also stored on the AAR's OT-37 record format. Changes made to these fields, via the UMLER record Line '1', generate revisions to Line '5'.

Date Built—Extracted from the UMLER record Line '1', Columns 31-34. All formats.

Date Rebuilt—Extracted from the UMLER record Line '1', Columns 35-36. All formats.

Date ICC Rebuilt or Secondhand—Extracted from the UMLER record Line '1', Columns 37-48. All formats.

Original Cost—Extracted from the UMLER record Line '1', Columns 43-48. All formats.

Cost Indicator—Extracted from the UMLER record Line '1', Column 61. All formats.

